

Meeting Agenda
Business Development Commission
January 9, 2019 6:30 p.m.
Hampshire Village Hall
234 S. State Street

1. Call to Order
2. Public Comments
3. Review of the December 12, 2018 minutes for approval
4. Update on the website for the village
 - A. Website data completed
 - B. Discussion on appearance (Mike please bring laptop for display on screen)
5. Downtown
 - A. Beautification Committee update
 1. Update on façade program roll out
 - a. Review applications pass by Beautification Committee
 - b. Vote and assign recommendation to Village Board
 - d. Review of matrix development (Fleury)
 2. Non-façade program spending
 - a. Discussion of newly found plans
 - b. Thoughts of beautification committee...any changes needed?
 - c. Can we recommend to the Village board as they requested.
6. Main Street Program
 1. Discussion on what we read
 2. Any thoughts on how we can use this
7. Company Profile
 - A. Copper Barrel next targeted for January
 - C. List of remaining companies
 1. Hampshire Pet Resort (Feb)
 2. Hampshire Pharmacy (Mar)
 3. Luxor Nails (April)

4. Tuscan Wine (May)
5. Hampshire Cleaners (June)
6. Stitches on State (July)

8 Survey

- A. Topics discussion –
 - a. What is the information we need from the people right now
 - b. Construct a list of 6 surveys over the next year
- B. David to present Parking lot survey results

9. Property Maintenance Plan

- A. Mike Armato to give report from meeting on the 8th
- B. Discuss the article provided by David

10. Joining ISCSC

- A. Ryan to discuss his research
- B. Do we request funds from the board to join

11. Grant writing consultant

- A. Board sought a proposal from someone (discuss the vote from the board)

12. Update on things going on in the village

- A. Blocks improvement
- B. Argentinian Wedding Hall
- C. Anything else we can share

13. Adjourn

Meeting Minutes
Business Development Commission
December 12, 2018
Hampshire Village Hall

Call to order at 6:40pm

Present: Commissioners Susie Kopacz, Eileen Fleury and David Pizzolato, Trustee Ryan Krajecki, and Village President Jeff Magnussen.

Meeting minutes from 11/14/18- David Pizzolato made a motion to approve minutes with a second by Susie Kopacz.

Website

Updates to the website are in progress. Working out some glitches. Direction was made to discuss progressing with development using Bryan Mroch.

Downtown

Beautification Committee Update

First application has been submitted from Roy's Place. The committee was able to discuss different options such as earmarking funds for quarterly or semi annual budget allocations. As the applications come in, the committee will review and give recommendations to the BDC.

Façade Program letters have been mailed to our Downtown businesses.

The committee gave direction to have the beautification committee create a long term street scape plan by the next meeting.

Mike Armato's suggestion to contact businesses that might be interested in the program prior to the letters going out was beneficial and key to the progress we have made so far.

Main Street Program

Susie reported on the research of the Main Street Programs:

Federal Program is basically just resources. There is a fee (\$350) to join and have access to those resources.

Illinois Program was taken away in 2015 the Governor. There was an award program offered in 2018, but the application deadline was April 2018.

Recommendation would be to table discussions till the new governor is brought into office to see if the program is reintroduced.

Action Item is for the committee to read through the mainstreet.org website with a recommendation to join or not by next meeting.

Company Profile

James Motors was profiled this month and had a very good response from the community.

Next Article will be Copper Barrel. Remaining companies to be showcased:

Hampshire Pet Resort, Hampshire Pharmacy, Luxor Nail Salon, Tuscan Wine, Hampshire Cleaners, Stitches on State.

Surveys

Parking survey is ready to post. It will be put on this evening and will run till after Christmas.

We will discuss more survey options to create a yearly idea of what information we would like.

Property Maintenance Plan

- A. Seminar in January-Mike Armato and Ryan Krajecki will be going and will report back.

Joining ISCSC

Recommendations to join the ISCSC. Ryan will make the recommendation to the village board at their next meeting.

The committee also discussed possibly contracting out some of the tasks we have in our future to companies that are more knowledgeable in those areas.

Potential Companies:

Blocks bought by The Patel family. Construction is under way. They have a great plan for

growth. Plan is to sell beer and wine as well.

Loves is well underway. Truck Country and Dayton Freight.

Stanley expansion 2 is almost done.

Pet Ag Building on Gast has broken ground. Looking at June/July for completion.

Corner lots on Ketchum and Big Timber have been purchased. Owners will be attending the meeting on December 18th for zoning hearing for a proposed banquet hall.

The old Palazzolo's building is being gutted and inside construction will begin for a proposed hamburger type restaurant.

Lazar property needs to complete some small issues with IDOT as far as traffic flow on 72.

Will be marketing for businesses to rent from his location.

Tom Gillespie has withdrawn his position in the committee. The Committee will look into finding another member to fill that spot.

Action Items:

Topics for future surveys

Read about Federal Mainstreet Program to see if we want to join

Read documentation on Property Maintenance

Ryan to look into ISCSC

Ryan to talk with Joe Lazar about his marketing

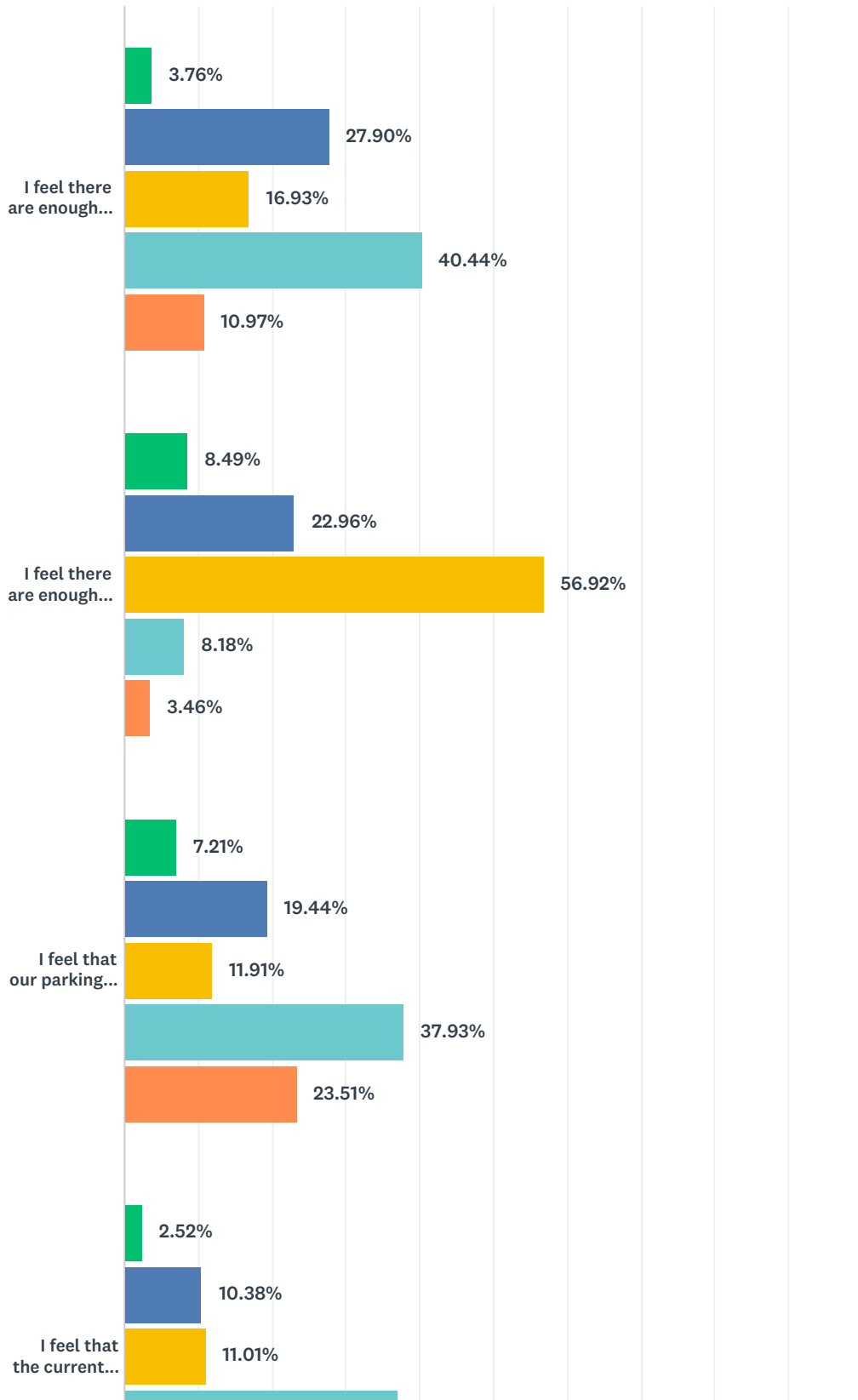
Come up with plan for non façade dollars.

Post the downtown parking survey (Pizzolato/Reid)

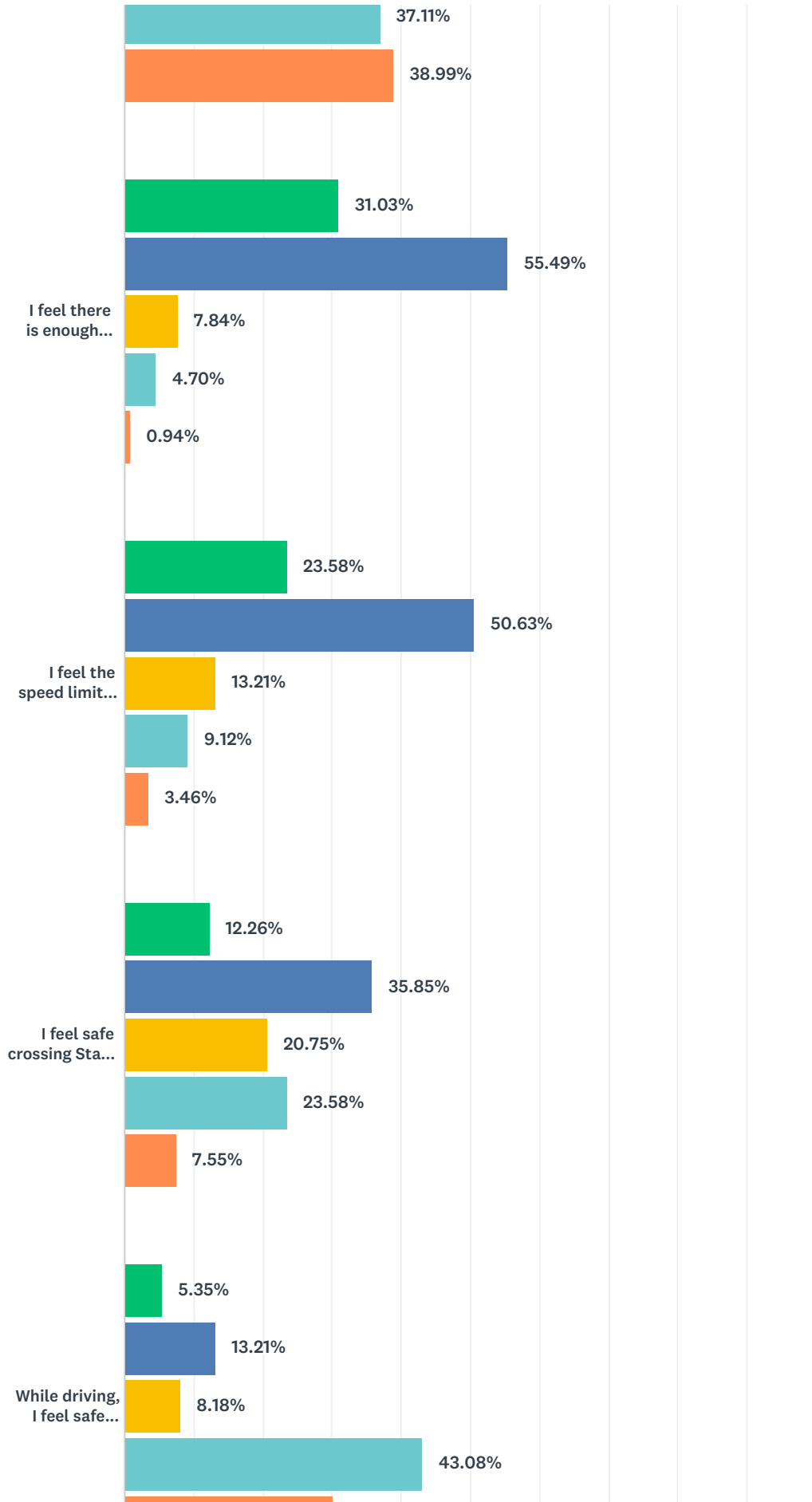
Motion to adjourn at 8:29pm by David and second Susie.

Q1 When thinking about downtown parking on State St., please rank how you feel regarding the following statements.

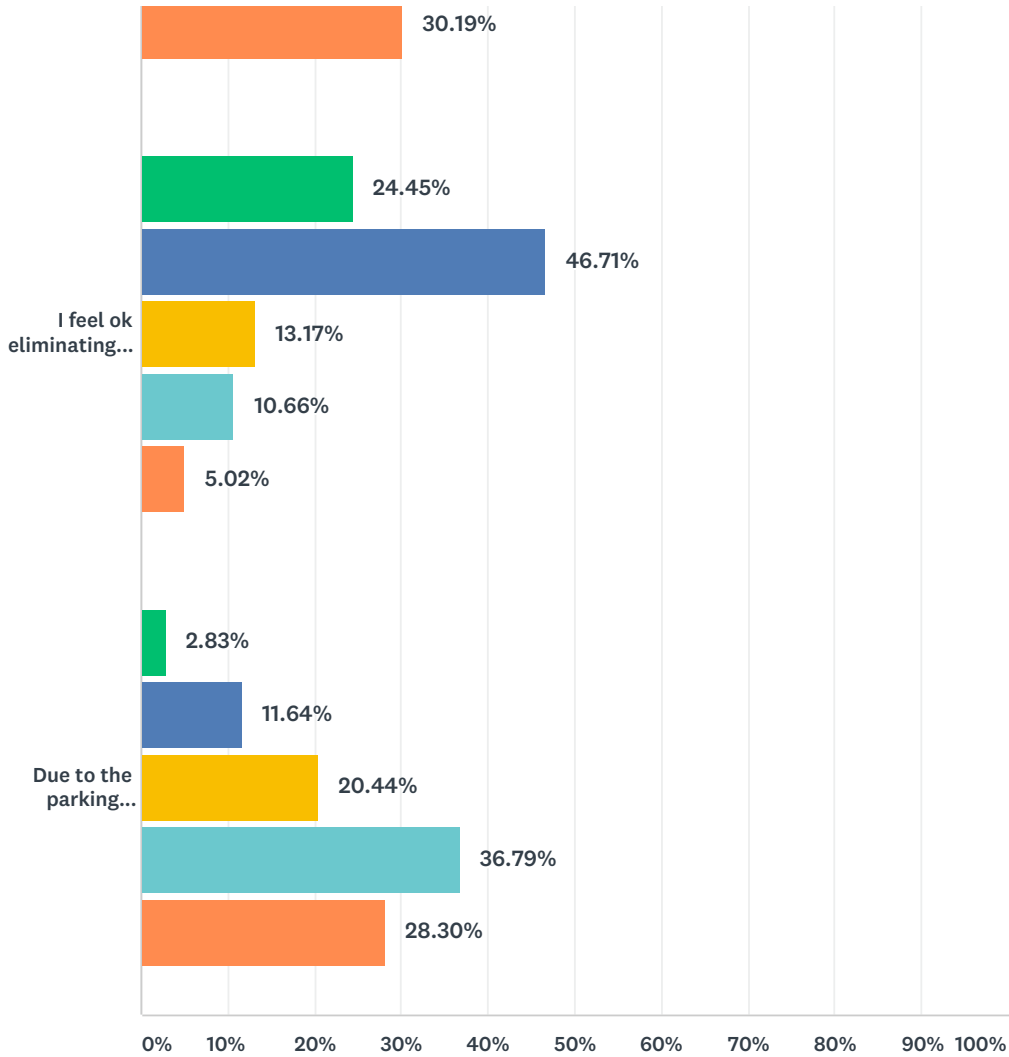
Answered: 319 Skipped: 0



December 2018 Hampshire Resident Survey - Downtown Parking



December 2018 Hampshire Resident Survey - Downtown Parking



■ Strongly Agree
 ■ Agree
 ■ Neutral
 ■ Disagree
 ■ Strongly Disagree

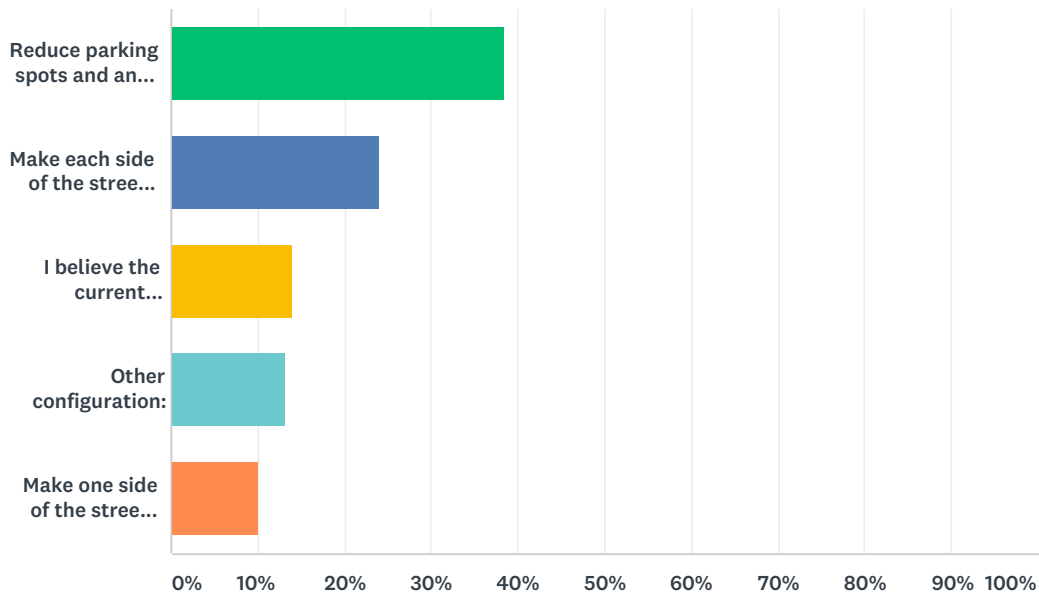
	STRONGLY AGREE	AGREE	NEUTRAL	DISAGREE	STRONGLY DISAGREE	TOTAL
I feel there are enough public parking spots	3.76% 12	27.90% 89	16.93% 54	40.44% 129	10.97% 35	319
I feel there are enough handicapped spaces	8.49% 27	22.96% 73	56.92% 181	8.18% 26	3.46% 11	318
I feel that our parking spot layout is ideal (angle, space width, space depth)	7.21% 23	19.44% 62	11.91% 38	37.93% 121	23.51% 75	319
I feel that the current parking space layout is safe (pulling in and backing out)	2.52% 8	10.38% 33	11.01% 35	37.11% 118	38.99% 124	318
I feel there is enough sidewalk space to walk safely downtown	31.03% 99	55.49% 177	7.84% 25	4.70% 15	0.94% 3	319
I feel the speed limit downtown does not need to be adjusted	23.58% 75	50.63% 161	13.21% 42	9.12% 29	3.46% 11	318
I feel safe crossing State St. downtown	12.26% 39	35.85% 114	20.75% 66	23.58% 75	7.55% 24	318
While driving, I feel safe turning onto State St. from a side street	5.35% 17	13.21% 42	8.18% 26	43.08% 137	30.19% 96	318

December 2018 Hampshire Resident Survey - Downtown Parking

I feel ok eliminating some spots to allow a better flow of traffic and increase safety	24.45% 78	46.71% 149	13.17% 42	10.66% 34	5.02% 16	319
Due to the parking situation, I don't even go downtown	2.83% 9	11.64% 37	20.44% 65	36.79% 117	28.30% 90	318

Q2 If the village were to look at redesigning the layout of the parking downtown, what do you feel the best configuration would be:

Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES
Reduce parking spots and angle each one at a bigger angle to allow more space for traffic driving through town	38.56% 123
Make each side of the street parallel parking	24.14% 77
I believe the current configuration is just fine	14.11% 45
Other configuration:	13.17% 42
Make one side of the street parallel parking	10.03% 32
TOTAL	319

#	OTHER CONFIGURATION:	DATE
1	Parking behind businesses within easy walking to main street businesses.	12/22/2018 5:30 PM
2	I would say more parking off of the Main Street. Parallel parking isn't an option because have you seen people try to parallel park?	12/22/2018 4:27 PM
3	Large Parking Deck with hopes Hampshire convinces Metra to use Rail for commuters to Chicago	12/20/2018 1:38 PM
4	Parallel parking only near the intersections of side streets. Reduce speed to 20 mph on State due to very limited visibility pulling on to State	12/20/2018 10:31 AM
5	Turning onto State St from E Jefferson is Unsafe. Can't see traffic to right or left when doing so. Needs to be reconfigured close to intersection.	12/20/2018 9:57 AM
6	Eliminate pick truck parking as they are to large and obstruct vision!!!!	12/20/2018 8:57 AM
7	Reduce the width of the sidewalks on each side of State Street. Bigger angling would help however it would eliminate much needed and already insufficient parking to support local business.	12/20/2018 4:49 AM
8	Bigger vehicles like large trucks need to be banned from parking downtown. Park on the side street or local parking lots.	12/19/2018 10:08 PM
9	No parallel parking, any other configuration that would allow better visibility for pulling in and out	12/19/2018 9:51 PM

December 2018 Hampshire Resident Survey - Downtown Parking

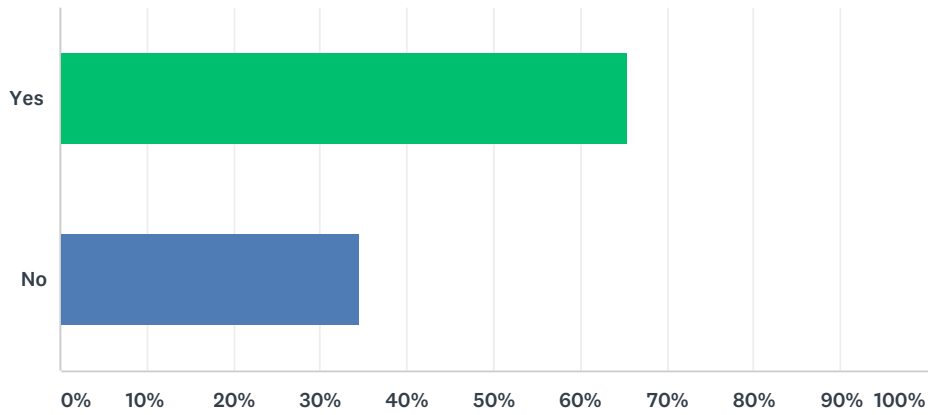
10	Consult an actual professional city planner to design this	12/19/2018 9:42 PM
11	Larger lot to park in near downtown area	12/19/2018 9:32 PM
12	Is there any land owned by the village that could be made into more public lots, plus the current configuration?	12/19/2018 8:47 PM
13	Make a parking lot similar to what most towns have	12/17/2018 7:39 PM
14	Keep it the way it is but don't allow giant pick up trucks and vans. Cars and SUV's Only. Impossible to back out when there are huge trucks with their back ends sticking out into state street	12/15/2018 10:55 PM
15	Make parking spots deeper	12/14/2018 3:01 PM
16	I have no suggestion but parallel parking would really reduce parking spaces.	12/14/2018 1:11 PM
17	I live downtown & pull onto State Street multiple times daily. I can't see traffic on State Street so pull out praying that it's clear. Whatever configuration makes it safer I'm OK with.	12/14/2018 12:22 PM
18	Just make them bigger 90% of the vehicles are SUVs, or trucks. To adapt to the future have electric charging spots. This will have a good effect on business because they need to wait for a charge, so they can shop and eat.	12/14/2018 11:04 AM
19	At the corners, make the handicapped stops parallel. This will allow better views for backing out of the diagonal spots. But it is the extra long trucks that are problematic. It is difficult if not impossible to see around them until the back of the car is out in the street. But how do you assign different parking spaces to the long trucks? Shrug.	12/14/2018 11:02 AM
20	Just enforce the speed limit to many speeders	12/14/2018 9:55 AM
21	Alternate parking in back or in a lot. Add signs so people know where to go	12/14/2018 9:09 AM
22	The spots by the beginning of the streets block visibility. There should be 4 way stops at Jefferson and state and state and Washington	12/14/2018 8:57 AM
23	Tear down Ace and provide a parking lot or have parking right in front of the train tracks across the street from the Library	12/14/2018 8:43 AM
24	Wider angles and a parking lot :)	12/14/2018 7:57 AM
25	Reduce parking spots and angle each one at a bigger angle. Also add more parking by the railroad tracks West of the Library.	12/14/2018 7:57 AM
26	Parking lot behind state street?	12/14/2018 7:20 AM
27	Don't take away parking, it will take away from business	12/14/2018 6:52 AM
28	Whatever can be done to make more room for two cars to drive down the street at the same time, and so that you can see when you turn onto state street from a side street	12/14/2018 6:43 AM
29	I'm not sure, but how it is now you cannot safely back out of a parking spot or turn onto state street safely from a side street	12/14/2018 5:20 AM
30	Unsure	12/14/2018 5:10 AM
31	Stop messing with everything!!! Leave it as is. Always have to mess with the downtowns. Pretty soon there won't be the country town feeling!!	12/14/2018 2:45 AM
32	I don't know the answer to this. It is sometimes hard to thread through State Street especially when there are pickups on one or both sides of the street. If a semi is coming the other way it becomes a bit harrowing. Parallel parking would allow more street space, but significantly reduce the number of spaces available, not to mention, lots of people aren't very good at parallel parking. The current configuration is quite dangerous. It is often nearly impossible to tell if you are about to pull out directly in the path of oncoming traffic.	12/13/2018 11:57 PM
33	Open new parking lots where old lots sit empty.	12/13/2018 11:21 PM
34	If able to make parking spot deeper so car/trucks don't stick out in the road.	12/13/2018 11:14 PM
35	I'm not sure what the best option is but it is really hard to see traffic from the side street	12/13/2018 11:02 PM
36	Can we do parallel parking and have a lot or two made? We also don't need the car dealership cars all over. That's ridiculous.	12/13/2018 10:47 PM

December 2018 Hampshire Resident Survey - Downtown Parking

37	Need more visual space for people pulling out onto state st. from a side street. You literally have to be almost in state street to see if vehicles are coming.	12/13/2018 10:46 PM
38	Make a public parking lot and allow for parallel parking in front of businesses on both sites of street. And add a sushi restaurant	12/13/2018 10:35 PM
39	I have no idea, but the sight lines coming from Washington or Jefferson onto State are absolutely terrible	12/13/2018 10:14 PM
40	I never learned how to parallel park! I'm terrible at it! Some towns have a separate lane to park (off the side.). I'll find an example and email you a pic!	12/13/2018 10:10 PM
41	Parking on one side only. Or, one-way traffic with parking on both sides. Also, signs to yield to vehicles backing out could help.	12/13/2018 10:07 PM
42	Both sides parallel, but then the lots and spaces need to be for public use only. Not for parking cars that are for sale at James motors.	12/13/2018 9:53 PM

Q3 Are you in favor of reducing/redesigning parking spots to allow for the sidewalks to be made wider to allow restaurant outside seating, benches, trees/flowers?

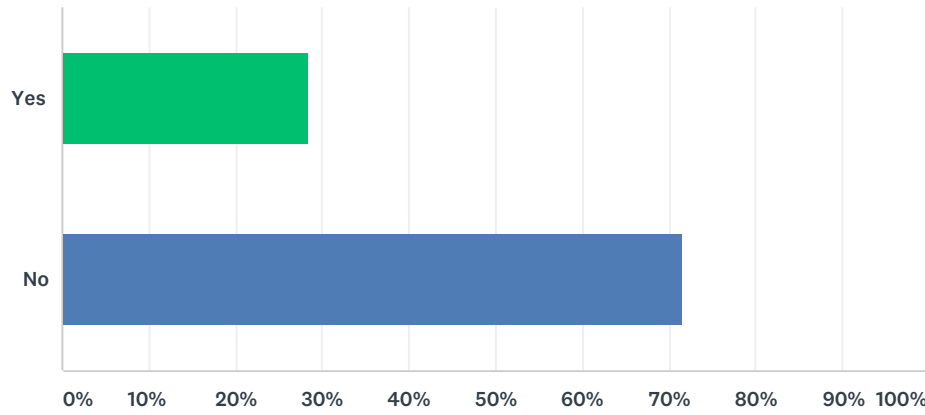
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	65.52%	209
No	34.48%	110
TOTAL		319

Q4 Are you in favor of putting time limits on parking spots downtown?

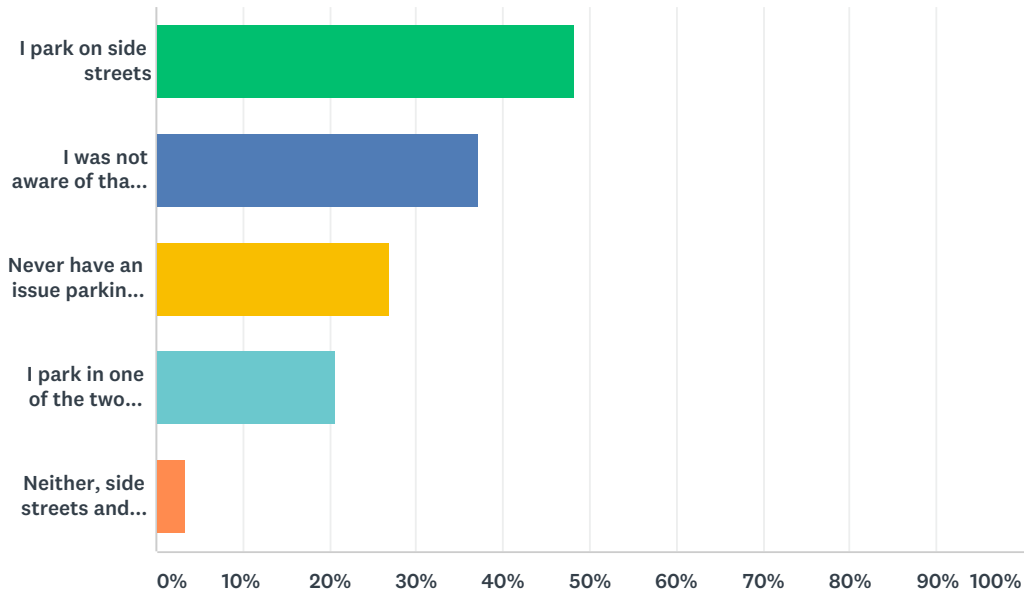
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	28.53%	91
No	71.47%	228
TOTAL		319

Q5 Do you ever park on side streets or use one of the two public parking lots downtown (Washington Ave. or Rinn St.)? Select all that apply

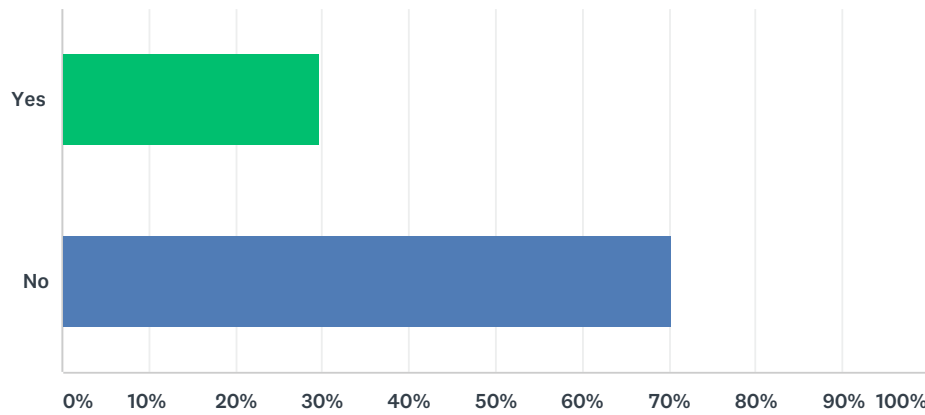
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
I park on side streets	48.28%	154
I was not aware of that there were public parking lots downtown	37.30%	119
Never have an issue parking downtown	26.96%	86
I park in one of the two public parking lots	20.69%	66
Neither, side streets and public parking lots are always full	3.45%	11
Total Respondents: 319		

Q6 While driving through downtown during a busy time, do you feel that there is adequate space between oncoming traffic and parked cars?

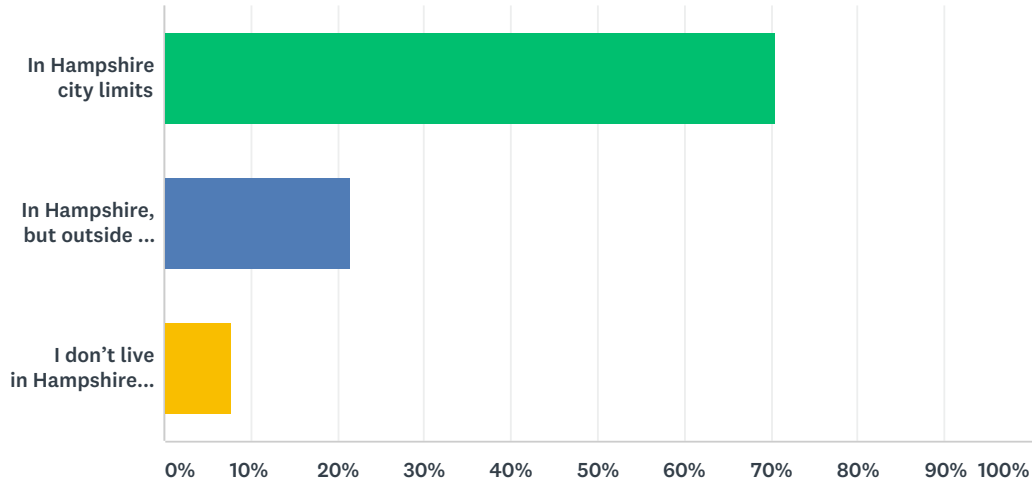
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	29.78%	95
No	70.22%	224
TOTAL		319

Q7 Where do you live within Hampshire?

Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES
In Hampshire city limits	70.53% 225
In Hampshire, but outside of town	21.63% 69
I don't live in Hampshire/I live in surrounding town (please list your town of residence)	7.84% 25
TOTAL	319

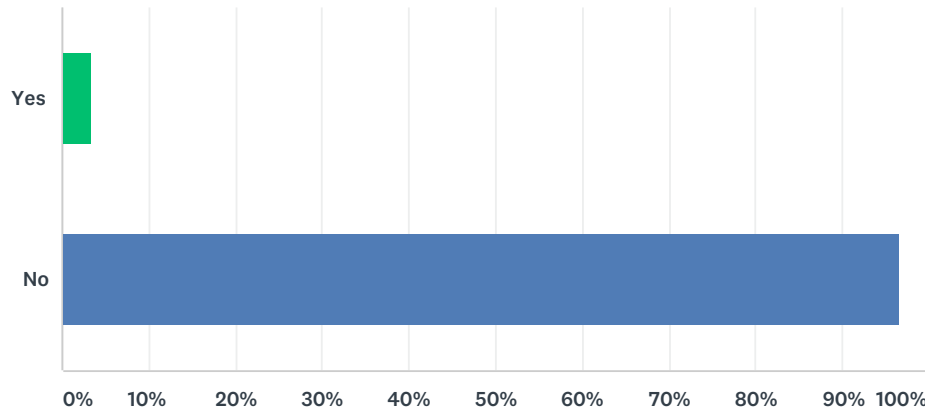
#	I DON'T LIVE IN HAMPSHIRE/I LIVE IN SURROUNDING TOWN (PLEASE LIST YOUR TOWN OF RESIDENCE)	DATE
1	Work in Town	12/22/2018 8:46 AM
2	Genoa	12/19/2018 11:03 PM
3	Belvidere	12/19/2018 10:18 PM
4	I live on State Street.	12/14/2018 11:02 AM
5	Sycamore, but I go to library 2x a week with my kids and frequent Blocks	12/14/2018 8:57 AM
6	Geneva	12/14/2018 8:40 AM
7	Batavia / lived in Hampshire 20+ years / visit often	12/14/2018 7:57 AM
8	I live in Gilberts, but grew up outside of Hampshire.	12/14/2018 6:21 AM
9	Belvidere	12/14/2018 5:33 AM
10	Pingree grove	12/14/2018 5:27 AM
11	Pingree Grove. But ALWAYS in town for school, sports	12/14/2018 2:45 AM
12	Burlington	12/14/2018 12:13 AM
13	Burlington	12/13/2018 10:40 PM
14	Pingree Grove	12/13/2018 10:23 PM
15	Pingree Grove	12/13/2018 10:19 PM
16	I grew up in Hampshire and my family lives there but I currently live in Algonquin	12/13/2018 10:15 PM
17	Pingree Grove	12/13/2018 10:15 PM

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18	Pingree Grove	12/13/2018 10:08 PM
19	Sycamore	12/13/2018 10:05 PM
20	Pingree Grove 20/72/47 (Sunset Cove)	12/13/2018 10:05 PM
21	Pingree Grove	12/13/2018 10:03 PM
22	Pingree Grove	12/13/2018 10:02 PM
23	Marengo	12/13/2018 10:01 PM
24	Pingree Grove	12/13/2018 9:59 PM
25	Elgin, but I work in downtown Hampshire	12/13/2018 9:55 PM

Q8 Do you currently own a business within the Village of Hampshire?

Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	3.45%	11
No	96.55%	308
TOTAL		319

Q9 If you answered "Yes" for question 8, please indicate any parking concerns you have regarding the parking downtown; otherwise, skip to the next question.

Answered: 33 Skipped: 286

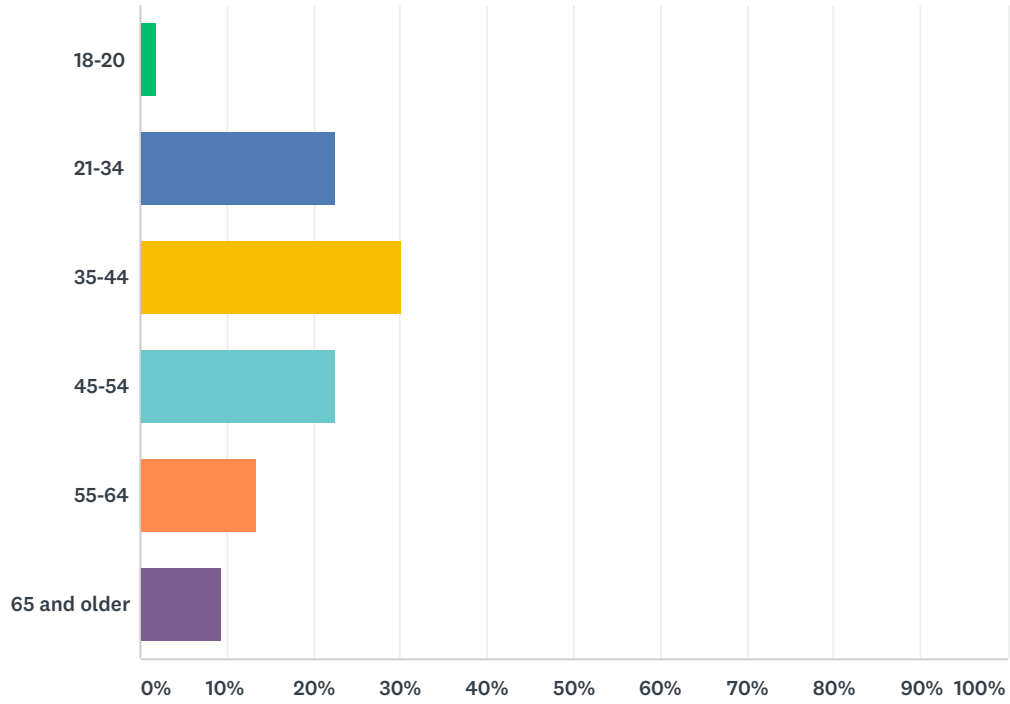
#	RESPONSES	DATE
1	It is so dangerous trying to pull out. I've even had my children go to the back seat to look out and see if there is on coming traffic since I can't see as I pull out when another car is parked next to me. I pull out very slowly and just pray that someone sees me and doesn't hit me as they are driving down State St.	12/22/2018 11:12 PM
2	Happy to see Copper Barrel thriving but it has definitely put a spotlight on poor parking if more thriving businesses are added.	12/22/2018 5:30 PM
3	Enforce speed limits, people driving too fast through town	12/22/2018 12:32 PM
4	Backing out from parking spot and entering State St. from side street is difficult. Trucks parking on corners block view.	12/22/2018 9:37 AM
5	Since, I don't have my business on State Street I can only talk about parking as a consumer. It can be difficult to cross the street when cars are parked next to an intersection because you can't see oncoming traffic. The same can be said for backing out of a parking spot. Reducing the amount of parking on State is going to be problematic especially with the car dealer using spots for inventory. I was told the other day that for the chamber's Xmas party they had 75 people. If there were 2 people per vehicle that's over 30 cars. If there were more businesses open at night parking would be an issue. Lane width for oncoming traffic is only a problem when extra long vehicles are parked on State. I believe U-turns into parking spaces is a problem too.	12/21/2018 5:41 PM
6	None. My professional work is performed in my home.	12/20/2018 1:38 PM
7	Any full-size pick up truck (especially dually's) should be prohibited from parking on downtown State. This would alleviate the space constraints for thru traffic.	12/20/2018 10:31 AM
8	Bigger sized vehicles should be banned from parking downtown. Park in parking lots or on a side street.	12/19/2018 10:08 PM
9	Na	12/19/2018 9:46 PM
10	N/A	12/19/2018 9:38 PM
11	Semis should not be allowed to drive through down town unless making a local delivery. Also the speed of vehicles exceed the posted limit	12/16/2018 1:36 AM
12	Skip	12/15/2018 2:42 PM
13	Na	12/14/2018 7:25 PM
14	Skip	12/14/2018 12:22 PM
15	Dna	12/14/2018 11:45 AM
16	None	12/14/2018 10:21 AM
17	Answered no	12/14/2018 8:43 AM
18	It's not safe with the amount of semi traffic through town.	12/14/2018 7:57 AM
19	ok	12/14/2018 7:22 AM
20	NA	12/14/2018 6:57 AM
21	N/a	12/14/2018 6:43 AM
22	No	12/14/2018 6:15 AM
23	Na	12/14/2018 6:02 AM

December 2018 Hampshire Resident Survey - Downtown Parking

24	I'm not in favor of reducing spots. There is already not enough parking downtown and people complain about having to walk so far.	12/14/2018 5:41 AM
25	Skip	12/13/2018 11:14 PM
26	N/a	12/13/2018 10:42 PM
27	No	12/13/2018 10:34 PM
28	Large pick up trucks parked downtown hang out into the street; I have had to swerve many times to go around the back end of a parked truck because they're too long for parking spots.	12/13/2018 10:32 PM
29	Angled parking sucks!	12/13/2018 10:32 PM
30	No.	12/13/2018 10:10 PM
31	Longer vehicles stick out into lanes at times	12/13/2018 9:59 PM
32	NA	12/13/2018 9:56 PM
33	The increase businesses downtown are great, but I see parking becoming a bigger issue. I am scared to pull off side streets and out of parking spots on State St. because you cannot see oncoming traffic.	12/13/2018 9:53 PM

Q10 How old are you?

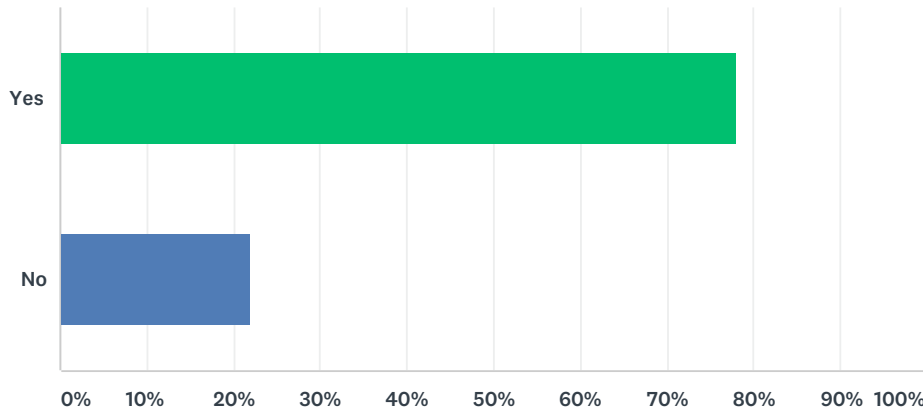
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
18-20	1.88%	6
21-34	22.57%	72
35-44	30.09%	96
45-54	22.57%	72
55-64	13.48%	43
65 and older	9.40%	30
TOTAL		319

Q11 Do you have kids?

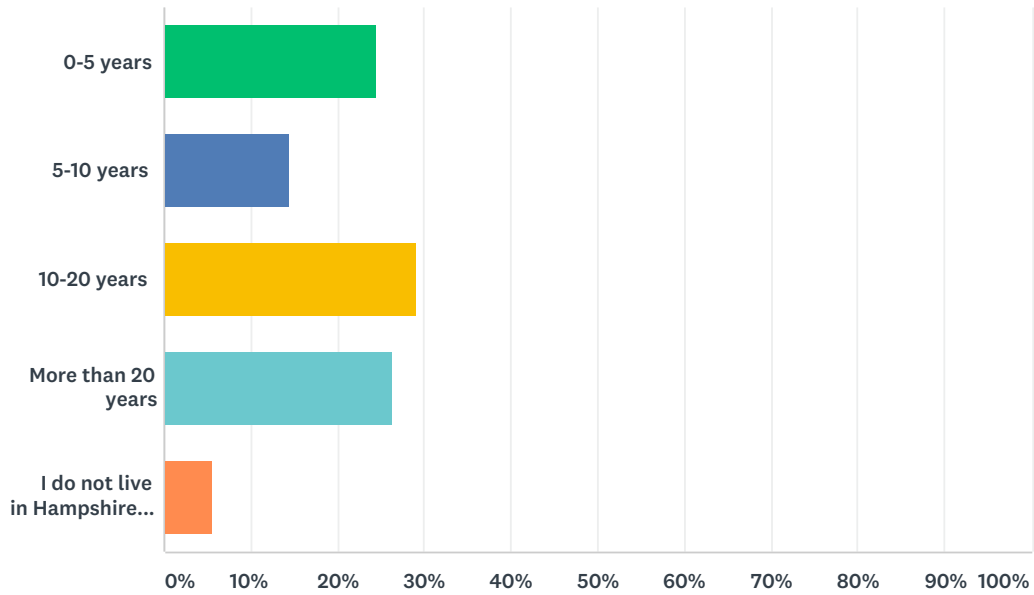
Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
Yes	78.06%	249
No	21.94%	70
TOTAL		319

Q12 How long have you lived in Hampshire?

Answered: 319 Skipped: 0



ANSWER CHOICES	RESPONSES	
0-5 years	24.45%	78
5-10 years	14.42%	46
10-20 years	29.15%	93
More than 20 years	26.33%	84
I do not live in Hampshire (please note the location of your residence)	5.64%	18
TOTAL		319

Q13 If you have any additional thoughts regarding parking within our downtown, please let us know.

Answered: 90 Skipped: 229

#	RESPONSES	DATE
1	Parking fine Need to enforce speed limits and people crossing the double yellow line to go around the person backing out of space	12/23/2018 9:08 AM
2	The parking in front of businesses can be a hazard to on coming traffic due to the lack of people not obeying the stop signs.	12/22/2018 8:11 PM
3	We might need to consider 4 way stops to improve safety and visibility.	12/22/2018 5:30 PM
4	Enforce and lower speed limit in town	12/22/2018 12:32 PM
5	I feel strongly that if drivers would exercise basic defensive driving skills when passing through our little downtown or parking on State St, which can be busy/congested at times, the City wouldn't need to even consider reconfiguring any of the parking situation. Additional signage at designated crosswalks could be helpful. Signage pointing vehicles to the parking lots could help as well.	12/21/2018 5:37 PM
6	The hardest thing is trying to back out-cannot see oncoming traffic.	12/20/2018 8:33 PM
7	There is plenty of room for passing traffic on State St if vehicles that are extra long do not park there----but that hardly ever is the case---long vehicles should park in the lots or get a ticket.	12/20/2018 5:29 PM
8	If you leave parking spots as they are now, I suggest no trucks, sub as they hang out to far and can't get around them!	12/20/2018 3:11 PM
9	It needs to be fixed	12/20/2018 11:37 AM
10	Need more enforcement of cars not pulling up fully into the parking spaces. Large trucks shouldn't be allowed to park on State Street given the current parking set up.	12/20/2018 11:11 AM
11	Given the current traffic and business conditions we are fine. However if the committee is looking to the future you will need to adjust the parking. The challenge will be paying for it AND convincing people it needs to be done.	12/20/2018 10:45 AM
12	The car dealership should NOT be allowed to park any inventory downtown.	12/20/2018 10:31 AM
13	The current parking configuration is a danger in many ways. Large pick-up trucks park and protrude into the traffic lane. That should be forbidden.	12/20/2018 9:27 AM
14	When a large truck or SUV parks in front of the library, you can not see well enough to back out. People come around the corner and you have to pray they are not coming as you back out.	12/20/2018 8:50 AM
15	The only issue I see is when pick-up trucks are parked in the spots. You can;t see to back out.	12/20/2018 5:56 AM
16	Maybe a couple spaces on side streets for larger trucks.	12/19/2018 10:07 PM
17	My biggest goal would be to increase visibility.	12/19/2018 9:51 PM
18	Na	12/19/2018 9:46 PM
19	Parking does not need to be addressed until more businesses/restaurants/etc. are in the downtown area.	12/19/2018 9:40 PM

December 2018 Hampshire Resident Survey - Downtown Parking

20	I would like to have more public lots if land is available with a walking path/sidewalks to State Street. It would be nice to have planters/benches on the sidewalks in the downtown. With the new restaurants and Vintage Hammer it would be nice for people to meet/gather while waiting for others to arrive or take a break to sit before or after shopping, or even get a meal to go and eat it outside. While it would be awesome to have sidewalk café seating, I just don't see how it would be possible on a large scale with current street parking. There seems to be a shortage of parking and all the spaces seem to be needed. Perhaps a few bistro style chairs/tables for warmer months. Could the alleys or lots behind the restaurants/shops (West side in particular) be used for parking and make a path(s) to the front of the buildings? On State St., parallel parking would hinder traffic too much with cars backing in trying to get in and out. I like the very slow speed limit through downtown, there is a lot to see with people and cars moving. Angled parking is better and easier to get in and out than parallel. When I go downtown, I like to park in the lot by the post office. I had no idea there was a public lot on Rinn. Is there a way to advertise off-street free parking? Perhaps only allow smaller cars to park nearer the street intersections for better visibility? Very few drivers obey pedestrians in the cross walks so it's hard to feel safe while crossing. Crosswalks are well-marked, drivers just don't obey. I wait there watching car after car go by ignoring me in the crosswalk until there is a break in the cars, then I cross. Looking forward to hearing more news about the downtown. Love the Vintage Hammer and Copper Barrel and am looking forward to the next new restaurant. Is there a place to advertise new developments downtown or get updates on your BDC work? A map of downtown (as well as 72/State) with businesses/services and related parking on the Village website? Something for people interested in visiting or considering a move to Hampshire can see? A flyer in a new resident packet, not only Chamber members, but all businesses? Thank you for working so hard to develop the downtown. We do try to shop local whenever possible and are inviting family and friends to come try out the new shop and restaurant. Keep the surveys coming! Thanks for listening.	12/19/2018 8:47 PM
21	Semis should not use state st unless making a local delivery. I see lots of them cutting through down town. Also many vehicles drive faster than the posted limit	12/16/2018 1:36 AM
22	The current parking makes it very hard to see as you pull out of side streets. Plus all the diagonal spaces makes downtown less attractive.	12/15/2018 7:29 PM
23	I struggle with the amount of space driving down state street when longer vehicles are angle parked. However part of me is overjoyed to see so many cars parked and visiting our local businesses. Another concern is when pulling on to state from a side street, without 4 way stops you really have to pull out on to state, thus blocking the crosswalks to have clear view before turning on to state.	12/15/2018 7:18 PM
24	Not enough public parking near downtown. Very hard to pull onto state st when parking is full.	12/15/2018 6:27 PM
25	Put a stop light at Jefferson and State and no angle parking within 250 ft of intersection so Jefferson traffic can see traffic coming down state.	12/14/2018 7:25 PM
26	Traffic goes too fast and when trying to pull out of a spot they will not slow down and blow their horns at you instead of allowing you to pull out.	12/14/2018 4:26 PM
27	it's hard to see backing out of a parking spot downtown. or pulling out from a side street	12/14/2018 3:55 PM
28	It is hard for me to see cars coming with the angled parking and when turning onto State St. from a side street.	12/14/2018 3:53 PM
29	Is it possible to reduce size of sidewalks therefore increasing street size	12/14/2018 1:11 PM
30	-	12/14/2018 12:22 PM
31	Need more street lights all over Hampshire. Very hard to see at night! Also more restaurants and possibly a shopping mall somewhere since the Huntley Outlet mall shut down.	12/14/2018 11:45 AM
32	EV parking would be great!	12/14/2018 11:04 AM
33	I think it could be questionable to think about redesigning sidewalks and parking for new businesses. Increase knowledge about off street parking. Who knew? When I drive on the south side of State, it appears to be new car parking everywhere. Good luck with this project.	12/14/2018 11:02 AM
34	Never able to see on coming cars. Thank goodness for my back up camera.	12/14/2018 10:51 AM
35	Keep our little town the way it is	12/14/2018 10:18 AM
36	I think parking is fine. Just need some 4 way stops in state because when parking spots are full you can not see driving traffic	12/14/2018 8:57 AM

December 2018 Hampshire Resident Survey - Downtown Parking

37	Install stop signs at the intersection where Rose Garden is	12/14/2018 8:43 AM
38	Restricting the size of vehicles that park on the corners might help with being able to see on coming traffic and around the corners.	12/14/2018 8:43 AM
39	The visibility from the side roads to turn onto Main street is terrible.	12/14/2018 8:38 AM
40	If there is a longer truck or van it goes out into the street and you have to go partially in the other lane to avoid it, I also find it hard to see cars coming when you are backing up. It's also hard for cars to see people trying to cross the street making it very dangerous. I won't park on the main strip when I have my kids with me if we have to cross the street because I find it too dangerous	12/14/2018 8:20 AM
41	Need to also consider future expansion for downtown when doing this	12/14/2018 8:18 AM
42	I would love to see town more attractive. (Decorations,lights, flowers, trees). Even more so than parking. Make it desirable to be proud of our downtown!	12/14/2018 8:01 AM
43	Add more public parking lots	12/14/2018 7:58 AM
44	I have always dislike having to pull out of a downtown street onto State street. Visibility is horrible and I always hope there are no cars blocking my view in the downtown windows so I can somewhat see oncoming traffic in them. If there are large trucks either parked or driving on State it can be quite crowded. I'd be all for angling the spots and removing a few for an overall safer State st.	12/14/2018 7:22 AM
45	no	12/14/2018 7:22 AM
46	Business needs to continue to grow downtown. There is a charm to the downtown, and what copper barrel did with their exterior really is in the right direction of what the downtown could be. Regardless, the parking situation will stagnate growth unless more people can be accommodated during the evenings, as to drive people to restaurants, and whatever else will come downtown.	12/14/2018 7:20 AM
47	A couple of key points: With the current parking arrangement full sized pick up trucks stick out into the traffic path. Backing out of spots are completely blind at times if a larger vehicle is parked next to you.	12/14/2018 7:09 AM
48	If you take away parking spots, it will be harder to find a open spot and I would be more likely to not shop downtown due to the frustration of having to drive around to find a spot. The parking lots are not convenient.	12/14/2018 7:04 AM
49	NA	12/14/2018 6:57 AM
50	Would be nice if the car dealership didnt Park it's cars on the side streets in public parking spots.... had to walk way down to eat at copper barrel bc State was full and then they took all the spots	12/14/2018 6:31 AM
51	The only issue is when you get the extra long super duty mega cab monster pick up trucks parking in the angle spots downtown. Then it becomes a hindrance when you're driving past because they stick out so far. That and Chrysler needs a different accommodation for storing their vehicles. Unless they're paying the village to store their vehicles all over town they shouldn't be allowed to do that.	12/14/2018 6:15 AM
52	Post signs that direct people to publicly parking lots.	12/14/2018 6:12 AM
53	Na	12/14/2018 6:02 AM
54	I think parking is fine. Time limits/parking meters might be needed as the downtown grows. I feel fellow motorists could be more courteous to those backing out or pulling in to downtown parking spaces, as well as to the pedestrians crossing streets.	12/14/2018 5:33 AM
55	Pulling from side street to Main Street, you can never see oncoming traffic, and with the new restaurant, it's now even worse, reposition parking would be safer for all, we're in the area at least 3 times a week for activities downtown, thanks for having a survey!	12/14/2018 5:27 AM
56	Parking should be parallel on the east side of the street. Worst intersections to pull out of with the most traffic seems to be the east side of state street, ie. post office, blocks, etc.	12/14/2018 4:38 AM

December 2018 Hampshire Resident Survey - Downtown Parking

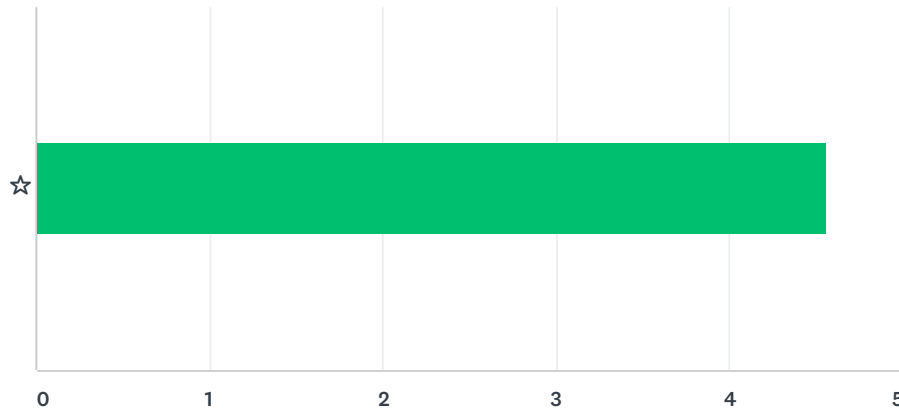
57	Thank you for taking the survey. When driving through downtown if there are larger trucks or big vehicles parked on both sides...it can get a little tight when there are other vehicles coming in the opposite direction. I find I have to almost cross the center line to allow enough room so as to not hit the back end of parked larger vehicles. This does get a little tricky with on coming vehicles. Turning onto State Street from a side street is simple dangerous. With the parked cars you cannot see if there is oncoming traffic. Many near accidents from cars trying to pull out and not being able to see. I hope something can be done with the parking downtown. Hoping this will help with the new businesses that are coming and allow people to park and drive through town safely.	12/14/2018 12:46 AM
58	Safety concern when turning from Jefferson Ave. onto State St. either way. If parking spots are filled up on both sides of State St. where Jefferson Ave. is, it is extremely hard to tell if there is oncoming traffic, and if it is safe to pull out and turn or not.	12/13/2018 11:47 PM
59	Parking garage off Washington Avenue.	12/13/2018 11:38 PM
60	Old, empty and rundown lots could be refurbished to be free public parking, with a "time limit" and allow state street to be mostly pedestrian friendly, rather than vehicles. Many small downtowns thrive the more open air the environment is. Thank you for your time :)	12/13/2018 11:21 PM
61	Is difficult at times going down state st when you have larger pick up trucks parked that stick out into the street.	12/13/2018 11:14 PM
62	Go to metered, parallel parking and add public lots	12/13/2018 11:12 PM
63	Cheap fix!!! Eliminate 1-2 parking spots in front of Rose Garden and First American Bank to open up the sight line as you pull out onto state street. Expensive: cut into the sidewalks to make the parking spots deeper and allowing larger truck to not block part of State st. (They stick out too far making people have to swerve around). Eliminate parking on the west side of state st and allow for open seating and shift the roadway by a foot or two.	12/13/2018 10:46 PM
64	Tell cops to stop writhing bullshit tickets for vehicles in peoples driveways. Maybe the said is a project and does see the streets	12/13/2018 10:35 PM
65	No	12/13/2018 10:34 PM
66	I think parking would be fine if it is for regular sized vehicles and not oversized trucks.	12/13/2018 10:32 PM
67	.	12/13/2018 10:32 PM
68	The angle parking is difficult to see around the cars when puling onto State St from a side street. It is lucky that the speed limit is 25 or there would be more accidents.	12/13/2018 10:19 PM
69	I believe Hampshire could use to look towards Main St in downtown Algonquin and would benefit greatly in overall township happiness and safety, while leaping into the current generation of architectural beauty through re-design! Downtown Hampshire has been mocked through great disappointment ever since I was in Middle School. It's a shame that there have been such few changes to Promote and encourage the younger generations gathering in and patroning the downtown area. I've been away for 15 years in the Army and truly thought someone would have stepped in to get this long awaited ball rolling faster than what shows and "seems to be".	12/13/2018 10:15 PM
70	Parking lot behind At & t	12/13/2018 10:15 PM
71	Present arrangement is unsafe. Pulling out from Washington or Jefferson is almost a blind proposition, sight lines are so poor. You have to pull way out to see around parked cars in order to see if there is oncoming traffic	12/13/2018 10:14 PM
72	Fully support beatification of downtown (e.g. widening sidewalks for outside dining, adding benches and trees/flowers).	12/13/2018 10:13 PM
73	The parking scares me bc its hard to see when trying to pull out as well as when your trying to turn onto state st.	12/13/2018 10:10 PM
74	My children are 14,1, and 2. I do not allow them to walk on or cross Main Street. We give them alternative routes to get to the library, Blocks, chick N Dip.	12/13/2018 10:10 PM
75	Land south of railroad tracks(west of state) that is currently mostly gravel, is ideal spot for additional parking. Elburn is a great example of parallel parking working as it should. Also would strongly encourage brick pavers in any or some of downtown sidewalk redesigns.	12/13/2018 10:10 PM
76	James dealership needs more parking, esp. for service dept.	12/13/2018 10:09 PM
77	I think the only time it gets tight is when people park their 8' bed, four door, f9000 30' long trucks, maybe a simple as cars only and compact cars or motorcycles closer to the corners	12/13/2018 10:09 PM

December 2018 Hampshire Resident Survey - Downtown Parking

78	I live at Oak & Jefferson and if I try to pull out onto Sate from Jefferson during peak dining time at the Copper Barrel I can not see past the parked cars if cars are coming. I have pulled out in front of cars numerous times. I try and go down the block to pull onto State but sometimes I forget and it's so dangerous. Maybe making the speed limit downtown 20 mph would help.	12/13/2018 10:07 PM
79	Making it easy for foot traffic and bicycles could eliminate some cars. Parking on one side only could really help. Or, one- way traffic through the few blocks of down town; traffic going the other direction would have to jog over a block through downtown.	12/13/2018 10:07 PM
80	The angle parking makes me nervous every time I back out.	12/13/2018 10:06 PM
81	People need to use the crosswalks! Parking on the opposite side of the street from where your going and darting across the street is not right!	12/13/2018 10:05 PM
82	Thank you for potentially doing something about this. Since we have the new restaurant and a lot more inventory from the dealership, parking and traffic tends to be scary because it is difficult to see when leaving your parking spot or crossing the street or even driving through downtown.	12/13/2018 10:04 PM
83	When pulling out of a side street I have to go very far into oncoming traffic just to see around a parked car. I think the slants need to change and better signage needs to be put out indicating where public parking is. I enjoy going to downtown Hampshire a lot	12/13/2018 10:03 PM
84	Pulling out onto state from a side street was always dangerous as you had huge blind spots. Would love to see the town figure something out	12/13/2018 10:01 PM
85	I like how the parking is in Hebron, il. For their downtown area.	12/13/2018 10:01 PM
86	None	12/13/2018 9:59 PM
87	The current angle parking configuration, creates many blind spots to to traffic and pedestrians when trying to cross or turn onto State St. I feel this is a dangerous and outdated parking configuration.	12/13/2018 9:58 PM
88	Signage on State St. directing to the public parking lots would be helpful. I've lived here 20 years and wasn't aware that lot on Rinn St. was public--thought it was a private business's lot.	12/13/2018 9:57 PM
89	No additional thoughts	12/13/2018 9:56 PM
90	The way the parking is now on State St, it makes it difficult to pull out of either Washington, Jefferson, Jackson or Rinn Streets safely onto State Street.	12/13/2018 9:55 PM

Q14 Thank you for your input and taking the time to share your thoughts. This is the first of several surveys and your participation will help us shape the future of all parts of the village to best meet your needs. A recap of the responses of this survey will be posted under the business development commission’s meeting minutes in the coming months. We will try and let everyone know once they are up. Feel free to rate this survey.

Answered: 180 Skipped: 139



	1	2	3	4	5	TOTAL	WEIGHTED AVERAGE
☆	0.00% 0	1.11% 2	6.11% 11	28.89% 52	63.89% 115	180	4.56



Prepared By:
Dave Pizzolato
Business Development Commission
The Village of Hampshire
December 22, 2018

2018 Downtown Parking Survey Results

As we have seen an large increase in the amount of activity within our downtown, the Business Development Commission wanted to understand the impact of our current parking situation. The commission invited residents to give their opinions and feedback regarding parking downtown, so we can make sure that the current infrastructure will support our business development efforts.

The survey was completely anonymous and 13 questions in length and took about 5 minutes to complete. We had 319 people complete the survey, which is a statistically relevant sample size representing approximately 20% of households within the village. 91% live in Hampshire and most of the rest of the questions (i.e. age, residency time frame, etc.) was represented across all segments.

What we heard...

- 76% of residents do not feel the current parking space layout is safe (pulling and backing out), with 65% claim they don't visit downtown because of the current parking situation.
 - Many indicated that most spots are not deep enough resulting in large vehicles sticking out into the street.
- 71% of respondents are ok with eliminating some spots on main street to allow for better flow of traffic and to increase safety
- Safety concerns were not limited to just pulling and backing into spots, but also turning onto state street from side streets. 73% of respondents do not feel that this is safe today.
- 85% are in favor of changing the current layout of the spots:
 - 39% - reducing the number of spots to angle each spot at a bigger angle to allow for more space to the traffic driving through
 - 34% - are in favor of making spots on both sides or one side parallel spots. Those that indicated this cited Genoa and Elburn as examples.

- 48% of respondents already park on side streets today, but 37% were not aware of the public parking lots on Washington Ave or Rinn St.
- The majority of respondents feel that changes to both the speed limit and timed parking spots are not needed.
- Most do feel that there is adequate space to walk on the sidewalk, but 66% are ok with widening the sidewalks to allow for additional space to allow for restaurant outside seating, benches and trees/flowers.

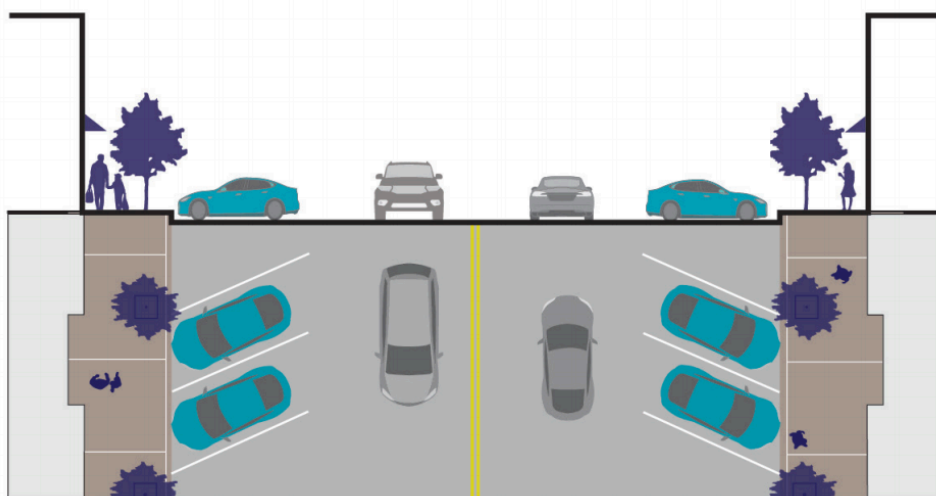
Commission Recommendation

Fundamental to the future plans and vision of Downtown to address the parking situation. As more businesses come here, the parking situation will only become worse. We feel our town is small enough at this point that those arriving by car can easily park their car one time and stroll to several destinations on foot before returning to their car.

We offer the following recommendations to make better use of the available parking supply and increasing the safety of those that travel to our downtown:

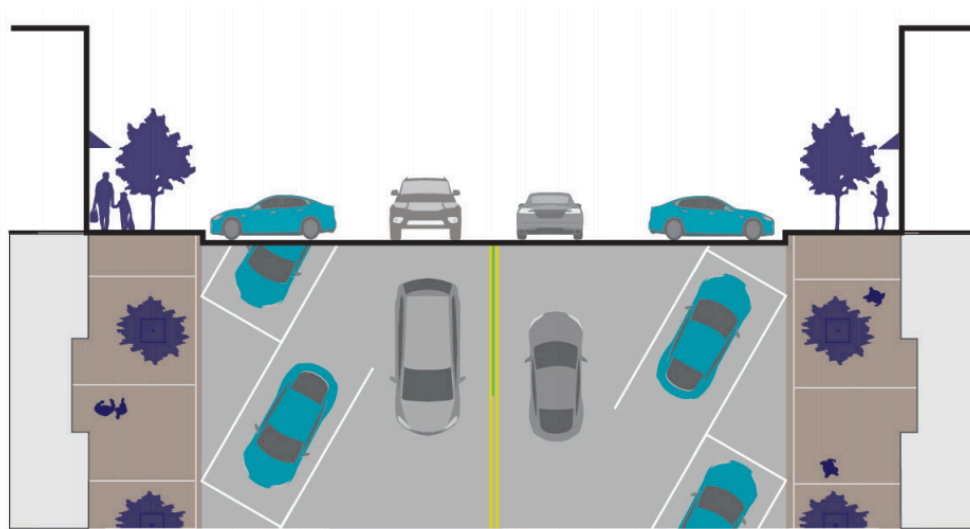
Redesign the Layout:

- All alternates presented have benefits and tradeoffs to their design. The purpose of the exercise is to provide feasible conceptual designs for further consideration:
- *Layout: Current 45-deg. (status quo)

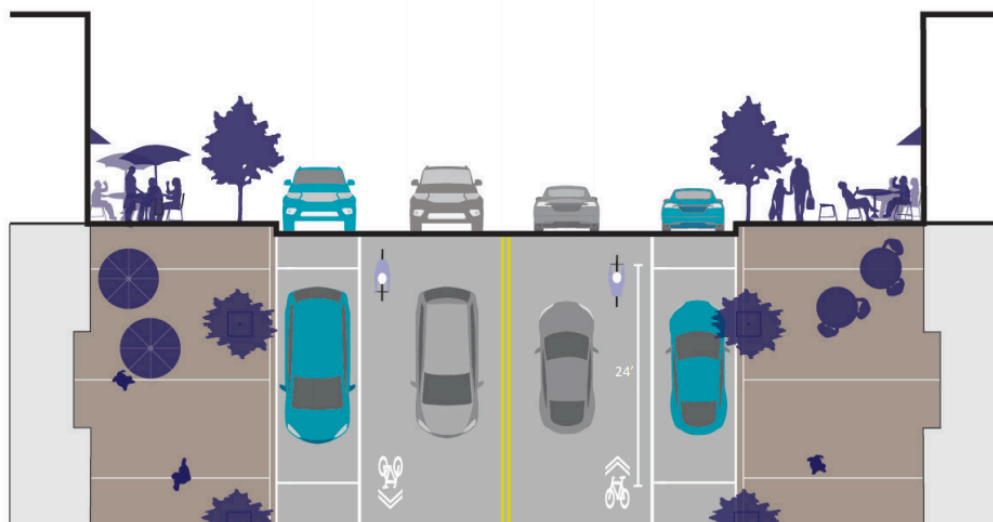


*<https://rdgusa.com/sites/waupaca/wp-content/uploads/sites/8/2016/03/2016-Parking-Study.pdf>

- *Layout: 30-deg. (improve backing out visibility/lose spots)

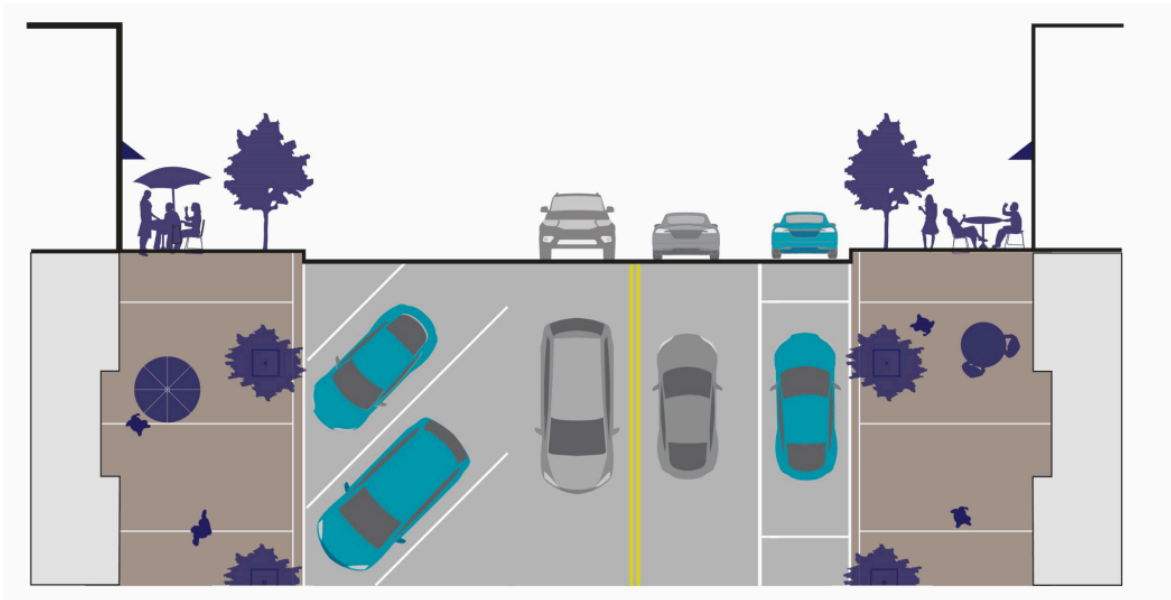


- *Layout: Parallel - Both Sides with no bike lane (improve overall safety for thru traffic and those turning onto State St.; sidewalks can be extended / lose parking spots)



[*https://rdgusa.com/sites/waupaca/wp-content/uploads/sites/8/2016/03/2016-Parking-Study.pdf](https://rdgusa.com/sites/waupaca/wp-content/uploads/sites/8/2016/03/2016-Parking-Study.pdf)

- *Layout: MIX Current & Parallel - One Sides with no bike land (improves the safety on one side of the street and those turning onto State St.; sidewalks can be extended on one side / lose parking spots, but not as many as other configurations)



Streets & Intersections:

- Establish buffers and layout to allow cars turning onto State St. from side streets enough visibility to turn out safely.
- Maintain high visibility crosswalks or install textured pavement elements in crosswalks.

Way-finding & Parking Lots:

- Create comprehensive directional and way-finding signage to direct those unfamiliar with the system to parking.
- Signage should clearly define usage of each public lot.
- Brand the parking system and incorporate Downtown identity.
- Develop online parking resources, such as a parking map, for residents and visitors.
- Explore opportunities for agreements to share parking in private parking lots

Future:

- As more specific development plans are finalized, the Village should reevaluate the projected adequacy of public parking and make additional changes as it sees fit.

*<https://rdgusa.com/sites/waupaca/wp-content/uploads/sites/8/2016/03/2016-Parking-Study.pdf>

CODE ENFORCEMENT

RECOMMENDATIONS FOR SMALL TOWNS



RURAL
PLANNING GROUP

UNDERSTANDING CODE

CODE: WHAT IS IT?

Codes are the parameters a city* places on what may be done and how it may be done within city or county limits. Cities and counties derive their authority to write and enforce code from the state constitution, their subsequent municipal charter, and statutes established by the state legislature; the charter outlines the authority of elected officials to manage affairs within the community through its code.

Code establishes how development may occur, requires care of personal property, and implements the goals and plans the city made in its general plan. It is important to note that municipal codes' authority comes from supporting the vision and goals in the community general plan and zoning map. If the codes do not have a direct connection to these broader documents, they can be viewed as arbitrary and create legal liabilities for communities.

*In this document "cities," "communities," and "municipalities" refer to towns, cities, and counties. Counties also create and enforce codes. The recommendations apply to all of these communities in a similar fashion.



ON-SITE ENFORCEMENT

All codes require enforcement. Codes that regulate how people use their personal property typically require city or county officials to visit site violations. The following are common code violations requiring on-site enforcement.

NUISANCE ORDINANCE

A nuisance can be almost anything, direct or indirect, that negatively affects other people's ability to use their property (e.g. loud music).

SOLID WASTE

Solid waste refers to garbage and debris. Having solid waste on private property is prohibited by most communities (e.g. junk cars and equipment in the yard).

ZONING

Zoning is included in code to allow for different uses. Businesses and/or residences in the wrong zone are common violations (e.g. unauthorized apartment).

ANIMAL CONTROL

Animal control ordinances address allowed types of animals, requirements for cleaning up after animals, noise, etc. (e.g. unauthorized farm animals).

SIGNS

The time, place, and manner of sign placement and use can be managed by communities through code (e.g. signs blocking walkways).

DANGEROUS BUILDINGS

Dangerous building code addresses health and safety hazards in homes, businesses, or planned additions (e.g. asbestos, unpermitted additions).



CODE ENFORCEMENT

BENEFITS

SENSE OF COMMUNITY

Community cohesiveness is closely associated with the look and feel of a community.

PUBLIC HEALTH & SAFETY

Code enforcement protects residents from potential hazards and health risks.

COMMUNITY & ECONOMIC DEVELOPMENT

Well-maintained communities attract tourism, new housing, and business development that evades poorly maintained communities.

PROPERTY VALUES

Property values stay higher when code enforcement protects neighborhoods from blight and other issues.

COMMUNITY IMAGE

Maintaining a clean community establishes a positive image of the community for residents and tourists alike.

PUBLIC WELFARE

Enforcement informs leaders of potential dangers and of residents who may need assistance in maintaining their property.

IMPLEMENTATION OF COMMUNITY VISION

Enforcing code is one of the primary tools a city has in implementing the long-term vision for the community.

QUALITY OF LIFE

A direct result of the benefits listed above, residents quality of life is directly tied to a city's decision to enforce its code.

DIFFICULTIES

MONETARY CONSTRAINTS

Most small towns and counties receive little revenue; this makes funding code enforcement difficult.

FEAR OF OFFENDING NEIGHBORS

Mayors and County Commissioners are not far removed from their constituents in small communities. Enforcing code on neighbors and friends is often personally difficult.

CONSISTENCY

Communities that have not enforced their code consistently in the past fear legal repercussions for starting to enforce now.

NO CAPACITY

Limited staffing raises the question of “what official or elected official has the time or know-how to enforce our code?”

NOBODY CARES (MOST OF THE TIME)

In many cases residents don't support enforcement until an issue directly affects them or their property.

POLITICAL CONSTRAINTS

Political leaders can be hesitant to support code enforcement when it causes frustration among voters.

The difficulties of code enforcement can seem insurmountable, leading communities to not enforce. This results in systemic problems that develop through long periods of not enforcing the code. As a direct result, residents do not gain the benefits of good code enforcement, and the systemic problems often become cyclical.

Simple enforcement programs can overcome monetary and capacity constraints. Enforcement strategies that incorporate public feedback and participation can help residents understand the positive aspects of code enforcement and garner public support for the code and its enforcement. Continuing to ignore enforcement will simply lead to increasing problems down the road.



STEPS TO GOOD CODE ENFORCEMENT

STEP #1: START AT THE PLAN

Good code enforcement starts long before city officials stand at a doorstep and ask a resident to clean up the half-dozen broken down cars in front of their home.

Good code enforcement must be based on good code and good code must be based on a good general plan.

Community plans must justify the provisions in the code which in turn justify the community in enforcing the code.

ACTION STEPS

1. Ensure your general plan accurately represents your community's long-term goals.*
2. Review the current code and zoning map to check for consistency with the general plan.*

*See worksheet at the end of this document

STEP #2: ENSURE YOU HAVE GOOD CODE

Good code does not mean *lots* of code. For most communities, having a thick code book can be more burden than blessing. Good code establishes guidelines that assist cities in reaching their long-term vision. Similarly, good code provides succinct, clear definitions of what is and is not allowed. City officials, especially the planning and zoning commission and city council,

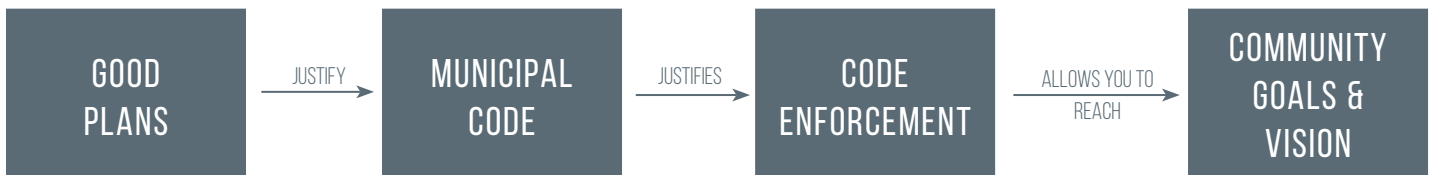
should be familiar with the code. Having complicated code can often lead to confusion rather than clarity in decision making. Simplifying code instead provides the public and decision making bodies with the clarity needed to understand what is and is not permissible.

ACTION STEPS

1. Review current code for consistency with plans and zoning.
2. Consider code revisions for sections that are unclear, or that do not support the community's goals, vision, or plans.
3. If serious inconsistencies exist, consider a code rewrite.

A **good general plan** clearly and consistently outlines the long-term goals of community members and leaders. Consistency throughout the plan will provide solid backing for code enforcement within the community. To help ensure plans are implementable, think about how the goals and vision will be reflected in the code and how they could be enforced as the goals are being written.





STEP #3: ENFORCEMENT PRECEDENT & STRATEGY

Cities often neglect code enforcement until there is a controversial disturbance. This can place communities in legally difficult situations because the sudden enforcement of code can appear arbitrary, curtailing a specific instance of a use, or targeting a single resident or disturbance.

To avoid this potentially dangerous situation, communities should consistently enforce their codes. If they do not, they will have more difficulty defending the code's enforcement when enforcement is most needed.

Communities should adopt an enforcement framework and strategy that outlines their intended means of enforcing the code. Enforcement methods vary significantly based on a community's size, resources, culture, and needs. Communities should take these factors into account as they select enforcement strategies. Enforcement strategies that match capacity and community culture will be most successful.

Community engagement is legally required for amending or adopting code. Community leaders should go above and beyond the legal requirements for public outreach before and after adopting major shifts in their code or code enforcement strategy and process.

Providing the public with information will help ensure residents know what is expected of them and can provide valuable feedback on how rules should be enforced.

Informing and asking for citizen feedback does not mean allowing citizens to make all the decisions. Community leaders still bear responsibility for final rules. Using resident feedback to inform the code's writing and enforcement process will help community residents feel more ownership of the code and more comfortable with its implementation.

ACTION STEPS

1. Evaluate current enforcement methods for gaps and opportunities to improve.
2. Conduct a public meeting soliciting public comment on preferred enforcement measures.
3. Establish enforcement plan improvements and ensure the public is aware of the code's requirements.
4. Evaluate how consistently you can conduct enforcement with time and monetary constraints.
5. Do not commit to more enforcement (faster response, consistency of patrols, etc.) than is reasonable for your finances or employee capacity.



CODE ENFORCEMENT BRAINSTORM

Enforcement in the real-world can be extremely difficult. Typically, questions surrounding who, what, and when sum up the key concerns leaders have about enforcement. The following are ideas that address these issues. Many of these strategies can be used at the same time, and the list is not comprehensive. Leaders should tailor these ideas to fit their communities needs and culture.

STRATEGY WHAT OPTIONS OR PROGRAMS EXIST?

The following list of strategies are in use or provide flexibility for communities in their enforcement strategies. Community leaders should recognize that incentive and assistance programs provide more flexibility than purely punitive enforcement strategies (fines and legal proceedings). Incentives and assistance tend to better match the culture of small towns. Incorporating multiple strategies into a comprehensive compliance plan is the best way to maintain the community's culture while ensuring code enforcement occurs.



STRATEGY	PROS	CONS	EXPLANATION
Community Incentive Programs	<ul style="list-style-type: none"> • Incentivizes all community members • Reward instead of punishment • Low public resistance 	<ul style="list-style-type: none"> • Costs money • Takes time to set up program • Potentially complicated • Creates expectations 	Incentive programs provide benefits to the community when specific goals are reached. For instance, Duchesne County, Utah provides raffled prizes to community members when the community has collectively removed a set number of broken down vehicles from private property. Everyone who donates a car is a potential winner.
Individual Incentive Programs	<ul style="list-style-type: none"> • Incentive can match cost of compliance • Directly assists those in most need • Reward instead of punishment 	<ul style="list-style-type: none"> • Potential abuse • Community resistance 	Individual incentive programs provide rewards to the property owner who has come into compliance. RPG recommends caution in forming individual incentives as they can encourage community members to be in violation in order to qualify for the incentive. These incentives (such as reduced taxes for a period or free dump passes) can be safely used for new property owners who come into compliance within a specific timeframe after purchasing the property.
Education Programs	<ul style="list-style-type: none"> • Reduces ignorance • Increases peer pressure to comply • Community-wide 	<ul style="list-style-type: none"> • Takes time • Outreach and materials cost money • Less direct impact 	Community leaders should engage with and educate community members on the reasons for the codes they hope to enforce. Taking time for public outreach to teach explain code requirements and why they exist reduces the threat of backlash when codes are enforced.
Community Clean-Up Days	<ul style="list-style-type: none"> • Reminds residents of need to clean-up • Allows town and residents to work together • Reduces barriers for compliance 	<ul style="list-style-type: none"> • Staff and/or volunteer time • Cost of supporting services (dumpster, lost revenue for landfill access, etc.) 	Community clean-up days provide free dump access and community outreach supporting clean-up in specific timeframes. Cities can provide and optimize use of the equipment and physical capacity required to clean up property that some community members may not have. These programs can help residents come into compliance when the primary barriers to compliance are physical or equipment related.
Assistance Programs	<ul style="list-style-type: none"> • Directly assists those in most need • Creates “no excuses” for violators 	<ul style="list-style-type: none"> • Significant burden for limited staff • Expectations from residents for services • Potential abuse 	Similar to community clean-up days, assistance programs are a great way to help community members who are incapable of complying with code requirements on their own. Some communities provide one time financial support to bring property in compliance. Ideas include using public works employees and equipment or having the city council organize service crews ² . These programs require a clear definition of who qualifies for the services.
Short-Term Loans	<ul style="list-style-type: none"> • Reduces barriers • Directly assists those in most need • Generates some revenue 	<ul style="list-style-type: none"> • Requires capital • Complexity of loaning • Issues with non-repayment 	For minor to moderate violations, communities can provide small, low interest loans to residents to bring their property into compliance ³ .
Vacant Property Tax Increases	<ul style="list-style-type: none"> • Incentivizes vacant properties to be inhabited • Generates revenue 	<ul style="list-style-type: none"> • Citizen resistance • Community culture • Punishment instead of reward 	Louisville, Kentucky increases tax rates for abandoned properties by as much as three times that of well kept properties. Other communities increase tax rates for perpetually non-compliant properties, increasing the rate annually until it reaches compliance ⁴ .
Vacant Property Fines	<ul style="list-style-type: none"> • Incentivizes vacant properties to be inhabited • Generates revenue 	<ul style="list-style-type: none"> • Citizen resistance • Community culture • Punishment instead of reward 	Cincinnati, Ohio charges fees for vacant property and increases fees year-over-year until the property is inhabited or sold ⁵ .
Fine Structures	<ul style="list-style-type: none"> • Flexibility • Recouping costs 	<ul style="list-style-type: none"> • Too steep of a fine structure can create citizen resistance • Punishment instead of reward 	Communities can use multiple sets of fine structures that are informed by the severity and frequency of an infraction. The cost of enforcement should be a primary consideration when setting fine schedules, however it is unlikely fines will cover the entire cost of enforcement. These can be assessed after a hearing (inferring criminal violation) or as an administrative citation where the enforcement authority can cite the violator according to administrative rule without first having a trial.
Criminal Classification	<ul style="list-style-type: none"> • Flexibility • Motivate exceptionally resistant non-compliers 	<ul style="list-style-type: none"> • Citizen resistance • Community culture • Punishment instead of reward 	Communities can classify most violations as a Class B Misdemeanor. In Utah, this allows for a maximum fine of \$1,000 and up to six months in prison. Some municipalities in the country even classify severe violations as felonies ⁶ . Communities must bring suit for the violator to be found guilty of criminal offense. RPG recommends taking violators to court as a last resort, when public health and safety are clearly endangered as a result of the violation and the violator has received multiple warnings without bringing their property into compliance.

CAPACITY WHO CAN HELP?

Various groups are able to enforce code, including contractors, volunteers, and employees. The following is a list of people and groups that can be considered as small towns set up their compliance and enforcement plan structure. Generally, the community's executive authority is expected to ensure code enforcement occurs.

PERSON / ORG.	WHO & HOW
Enforcement Resource Sharing	Several communities in northern Utah currently share the cost of enforcement by having multiple jurisdictions pay a contractor (in many cases the local sheriff's office) to cite violations in their jurisdictions. Other communities also pay local law enforcement to issue fines and clean up violations.
Community Institutions (Churches, Clubs, etc.)	Local scout troops, churches, and service organizations are great resources for helping residents with trash, cars, or other blight problems. These local institutions can increase the number of residents aware of clean-up days and code requirements. They also reduce barriers for citizens resistant to city interference with their property. Public works vehicles and volunteers vehicles can be used in partnership with these institutions to maximize participation.
Citizens	Citizen complaints can be the primary information source for violations in the city. Online submission forms tied into community websites provide a low-cost means of collecting citizen complainants addresses, contact information, dates, and concise descriptions of violations before the complaint is submitted. These systems require a community to make someone available to follow up on complaints.
Planning & Zoning Commission	Planning and zoning commission members should know the code and the general plan; this knowledge makes them ideal candidates for an enforcement team. This can be done with compensation for commission members or pro bono. Flexibility should be provided to these groups to institute legal, yet creative, enforcement programs.
Neighboring Communities	For communities with no capacity to pay, trading services with neighboring communities can reduce costs and ensure enforcement. In this scenario, two neighboring cities would provide code violation warnings and fine violators for each other. This allows a third party to cite violations which may reduce interpersonal tensions in small towns. It also makes high-school sports games more interesting.
Private Company on Retainer	Similar to enforcement sharing, communities can put their code enforcement on retainer to a private entity. This provides arms-length, unbiased enforcement and helps ensure consistency across the community.
Citizen Inspectors	Cities can educate specific residents on the municipal code and train them on the cities enforcement procedures. The city can then allow these citizen inspectors to patrol their neighborhood, document violations, and begin the code enforcement process. Belligerent or repeatedly noncompliant cases should be referred to city officials ⁷ .

Relationships developed between code enforcers and community members are consistently recognized as the most important components of good enforcement. When enforcers know and can work with community members to come into compliance, enforcement helps solve, rather than create, problems for community members.





CODE ENFORCERS SHOULD:

1. Know the code
2. Understand their authority to enforce
3. Understand the reasons behind the code they enforce
4. Work *with* residents to help them comply
5. Enforce code equitably to all properties and property owners in the community

FREQUENCY WHEN SHOULD I ENFORCE? HOW OFTEN SHOULD I PATROL?

Enforcement should be conducted when there are violations (whether identified by citizens or community officials). Citizens don't always know what constitutes a violation or don't want to report their neighbors violations. City officials also rarely know everything occurring in their community. For these reasons, community inspections help inform enforcement needs.

Consistency is the most important consideration for enforcement over time. Available personnel and capital and the severity of code enforcement problems within a community are key considerations for developing an enforcement timeline. Based on these considerations, communities can create a compliance plan that incorporates specific enforcement strategies, who will carry out these strategies, and how frequently enforcement patrols or surveys will take place. Ensure your city has the manpower and budget to enact their compliance plans timeline.

Several communities along the Wasatch Front elect to only respond to resident complaints and blatant violations recognized by city officials; others have full-time enforcement specialists who patrol the city as their full-time job. In most small towns, a patrol that occurs once a year or every six months is sufficient to recognize new violations and begin enforcement procedures. The violations cited in these patrols require immediate follow-up to support and enforce compliance.

A community clean-up day is a great precursor to code violation patrols. Having the patrol after a clean-up day ensures residents have been provided a reminder and, in some cases, assistance to come into compliance. When a property is noncompliant after a well executed clean-up day, additional efforts will be necessary to ensure compliance.



BUT WHAT IF . . . WE HAVEN'T BEEN ENFORCING OUR CODE?

Communities that have not consistently enforced their code have placed themselves in a difficult situation, but continuing to not enforce code will only make things worse. Communities who find themselves in this situation should consider the following steps:

1. Conduct a general plan review, ensuring the plan accurately represents the residents' long-term desires for your community. If it does not, a general plan revision is needed.
2. Review the current code and evaluate how well it implements the vision laid out in the general plan. If the code does not support the plan, consider revising or hiring consultants to re-write the code to align with the general plan.

If the plan is acceptable, and the code supports the plan, communities should:

1. Develop a code enforcement plan.
2. Present the enforcement plan in a public meeting.
3. State that the city intends to enforce consistently moving forward.
4. Accurately enforce the code in perpetuity.

These steps cannot completely protect communities, however they do provide the basis for justifying a fresh start in enforcement and will ensure community members are aware of, and have the opportunity to comment on, expectations established in the code. It is not possible to enforce with 100 percent accuracy. Rather than waiting to have the perfect process or perfect code, leaders should ensure the code is viable and start enforcing at a pace that is feasible for the community in the long-run.

SAMPLE: ENFORCEMENT PROGRAM

ACTION	RESPONSIBLE	Jan	Feb	Mar	Apr	May	Jun	Jul	Aug	Sep	Oct	Nov	Dec
Plan audit*	Planning Commission	█											
Code audit*	Planning Commission		█	█									
Public meeting	Mayor			█									
Code compliance officer training	Planning Commission				█							█	
Community clean-up day	City Council				█							█	
Town-wide violation patrol	Code Enforcement					█							█
Complaint response	Code Enforcement	█	█	█	█	█	█	█	█	█	█	█	█

*Every five years. This is simply a generic recommendation. Cities should consider their rate of change in determining how often to review.

This sample enforcement plan is intentionally simple. It focuses on two community clean-up days, where clean-up occurs community wide, with assistance from public works. Most enforcement programs in very small towns should stay simple, and ensure they do not over-extend the town’s capacity to enforce. In this example, any empowered group could implement the enforcement program. This model would be carried out as follows:

- **Plan & Code Audit.** Every five years, the planning commission will audit the general plan and code, ensuring the plan still matches community goals and that the code serves to accomplish the goals in the general plan.
- **Public Meeting.** Each March the Mayor and City Council will host a public meeting, in which they will briefly explain the code’s requirements, the reasons for the code, and a brief explanation of how code is enforced within the city, including advertising the upcoming community clean-up. Treats should be provided.
- **Compliance Training.** Following the public meeting, the code enforcers (consisting of one council member, public works employee, and the city recorder) will be retrained on city code and proper code enforcement procedures.
- **Community Clean-Up Day.** A semi-annual Community Clean-Up Day will occur in the first two weeks of April and November. One free pass to the local landfill will be provided to each residence (large trailers could be used when landfills are not available).
- **Community Compliance Patrol.** During the two weeks following the clean-up day, city officials will patrol the community, providing notice to property owners of violations.†
- **Complaint Response.** During the rest of the year, the city will respond to citizen complaints via the city website’s code violation referral page.

† All notice of violations will follow the city’s enforcement model, with the maximum penalty being a class b misdemeanor for gross neglect and an unwillingness to work with city officials to remedy violations (see next page for example).



Working with local institutions, like churches, clubs, and political groups, to get the word out about enforcement measures, “clean-up days,” and other information will improve the turnout and results of city efforts to bring residences into compliance.

SAMPLE: ENFORCEMENT PROCESS

There are two primary methods of code enforcement: offering incentives or issuing penalties. This sample methodology incorporates an incentive/assistance program into a more traditional, penalty based system. Incorporated communities can adopt any process they deem valuable, provided it does not violate residents constitutional rights.

THIS MODEL *

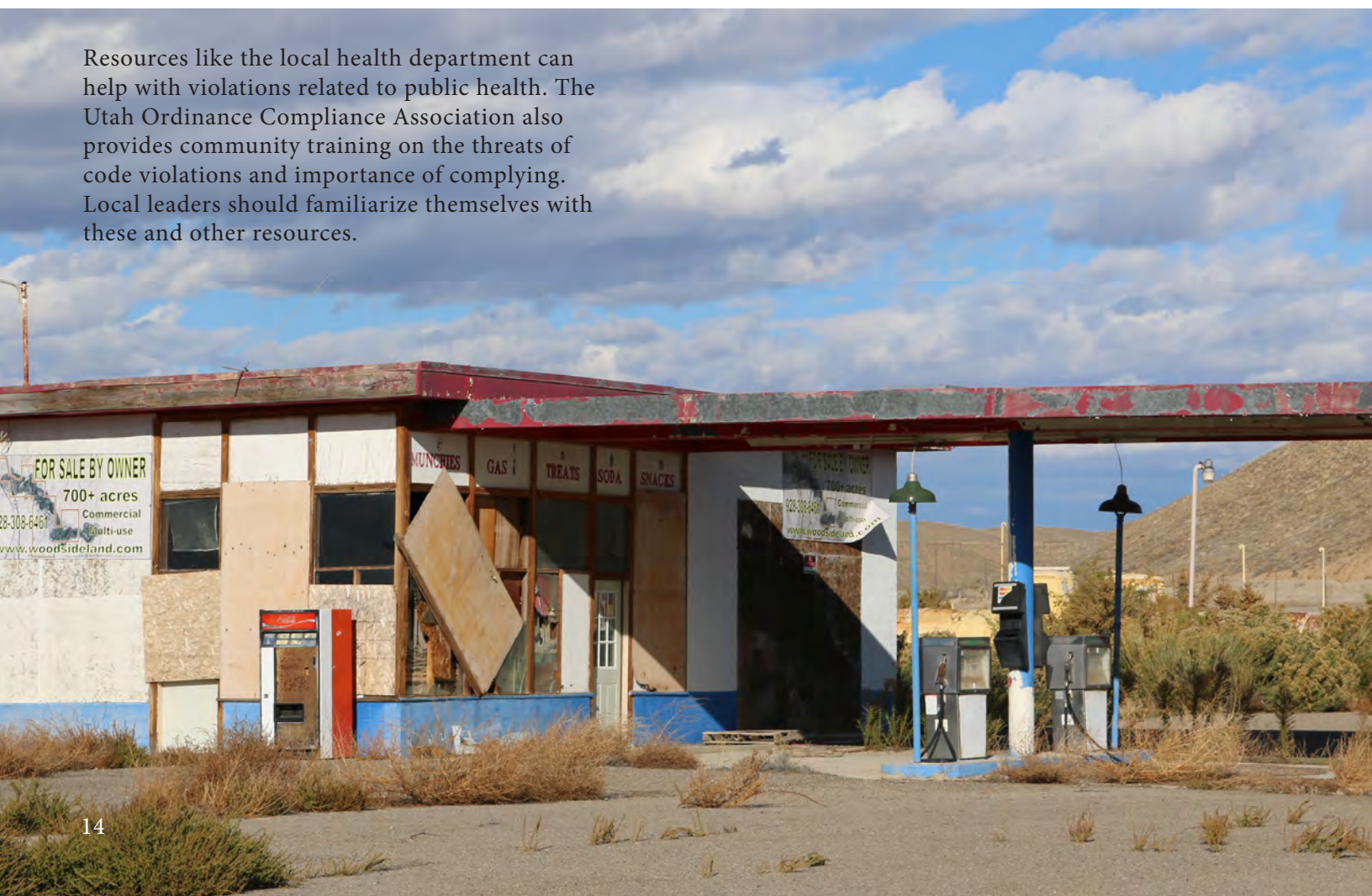
In this model, a violation is observed and documented. Based on the nature of the violation, the resident is informed of their noncompliance with a written warning or notice of violation (with reference to the violated code). The city can then offer assistance or an incentive to come into compliance, or provide a warning of pending legal action and potential fines. If

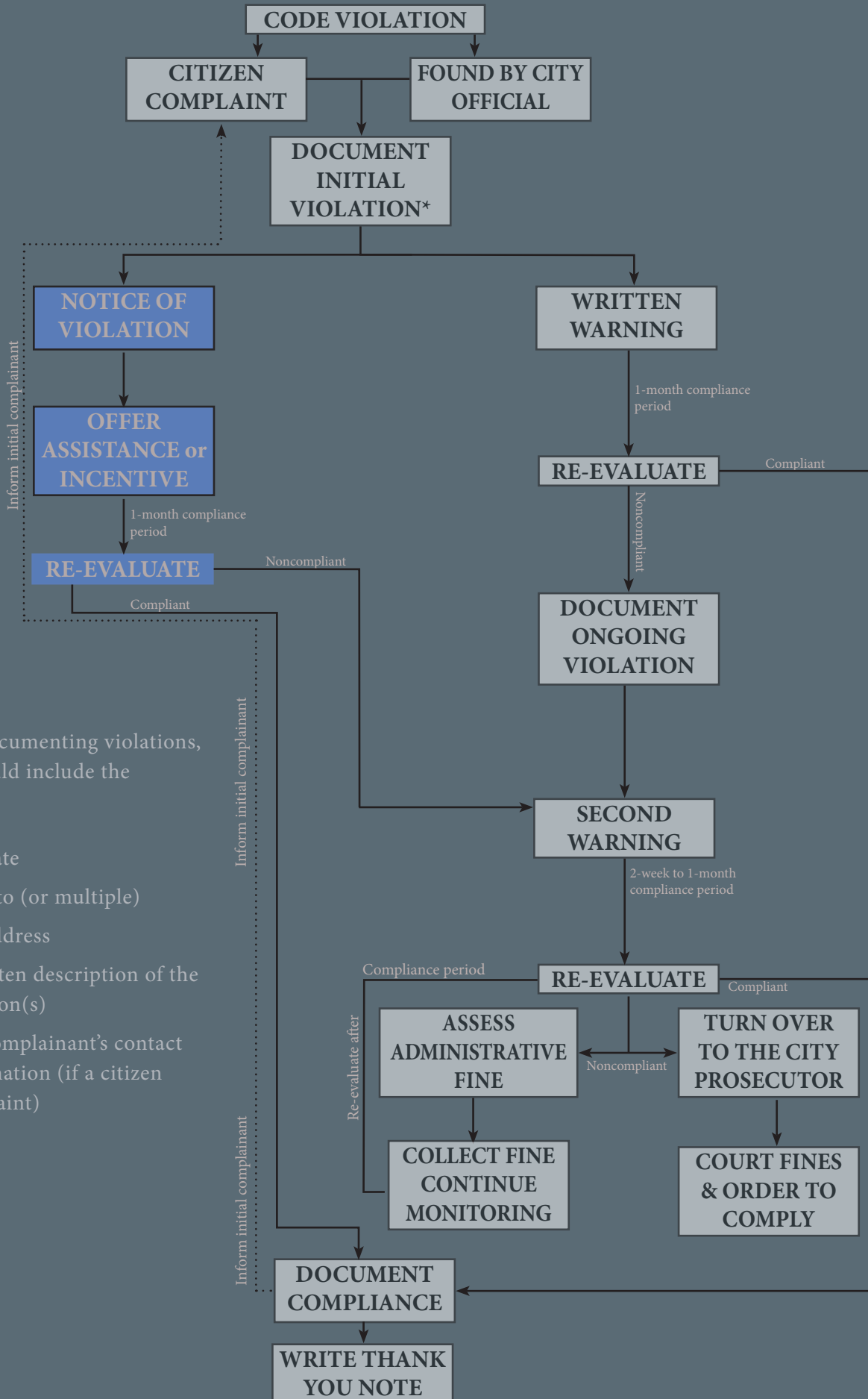
the resident does not take advantage of the assistance or incentive, or respond to an initial warning after a one-month compliance period, the ongoing violation will be documented. Then, a second warning will be issued, illustrating the city's intent to fine or seek a court ordered injunction to comply. After a two-week to one-month additional compliance period, the property will be re-evaluated, and an administrative fine will be assessed or the case will be turned over to the city (or county) prosecutor.

When compliant, the city will document compliance, issue a notice to the resident who made the initial complaint, and write a thank you note to the resident who brought their property into compliance.

*See Works Cited reference 8 for the sample enforcement procedure that informed this model.

Resources like the local health department can help with violations related to public health. The Utah Ordinance Compliance Association also provides community training on the threats of code violations and importance of complying. Local leaders should familiarize themselves with these and other resources.





*When documenting violations, cities should include the following:

1. The date
2. A photo (or multiple)
3. The address
4. A written description of the violation(s)
5. The complainant's contact information (if a citizen complaint)

Knowing the difference between a good or bad plan element can be difficult. Having multiple people analyze the plan can help identify areas of common concern. The planning commission should evaluate community plans with this document.



SAMPLE: PLAN & CODE AUDIT

The Rural Planning Group (RPG) uses this code and zoning audit to evaluate communities' codes, plans, and zoning maps. This audit considers many of the legal requirements (as per state laws) for municipalities' general plans, zoning, and code. This audit also assesses a community's plan and code consistency. Each area is scored based on a "red," "yellow," or "green" scale. Red indicates a community does not have the provision, yellow indicates the provision exists but is lacking, and green indicates that the provision is present and adequate. Additional criteria that address issues specific to individual municipalities are recommended as add-ins to the audit. Planning commissions or other city officials should be capable of completing this audit.

As community leaders complete this assessment, they often ask, "Well what do I do now?" The findings of this audit need to be shared with the elected officials who have the authority to create or authorize changes to the plan, map, and code. If things are in serious disarray, RPG recommends taking a hard look at updating or re-writing the community's general plan and orienting the code to the goals set forth in the general plan. This will ensure the community's code and zoning have a bearing towards the community's long-term ambitions.

PLANNING & ZONING ORDINANCE AUDIT

ITEM <small>Required by State code... (State code reference)</small>	CONDITION	NOTES
Do we have a General Plan? (10-9a-401)		
Do we have an official map? (10-9a-401, -407, 10-9a-103(34))		
Do we have a zoning ordinance? (10-9a-502)		
Do we have a zoning map? (10-9a-502, 505)		
Are our plans and ordinances publicly available?		

PLAN ELEMENTS Does it cover...

Transportation (10-9a-403)		
Affordable housing (10-9a-408) <small>(Towns are exempt from this requirement)</small>		
Land use (10-9a-403)		
Implementation strategy <small>(best practice)</small>		
Capital improvements plan <small>(aligned to GP 10-9a-406)</small>		

ORDINANCE REQUIREMENTS

Creates a Planning Commission (10-9a-301(1)(a))		
Establishes an appeal authority (10-9a-701)		
Residential facilities for elderly/persons w/disabilities (10-9a-516)		
Allows for compliant manufactured homes (10-9a-514)		
Addresses cell towers (can't prohibit)		
Reestablish nonconforming structure after calamity (10-9a-511)		
Allows for charter schools (10-9a-305)		
Allow for adult-oriented businesses		

PROCEDURES for...




Planning Commission (10-9a-302)		
Land use authority (10-9a-103(24))		
Appeal authority (10-9a-701)		

PUBLIC NOTICE

Have they been posting to the Utah Public Notice website?		
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GENERAL PLAN & CODE CONSISTENCY Do our plan and code work together to the community's ultimate goals?

Overall, how well are city goals and vision reflected in the code? <i>List community goals from each section of your general plan and review city code with each goal in mind. Using green, yellow, and red, describe if and how well current codes establish each goal.</i>		
---	--	--

-  The requirement is fulfilled and in good condition
-  The requirement is fulfilled but is in questionable condition
-  The requirement is not fulfilled



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- 2 Bracco, Frank. "An Incremental Approach to Improving Code Enforcement and Compliance in Clayton County, GA." Carl Vinson Institute of Government. 2010.
- 3 Ibid.
- 4 City Auditor's Office. "Performance Audit Neighborhood Preservation Division." City of Auditor's Office of the City of Kansas City, MO. September 2012.
- 5 Bracco, Frank. "An Incremental Approach to Improving Code Enforcement and Compliance in Clayton County, GA." Carl Vinson Institute of Government. 2010.
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- 10 Greensboro Budget and Evaluation Department. "Code Compliance Benchmark Study." Budget and Evaluation Department Greensboro, NC. Appendix C. November 2012. <<http://www.greensboro-nc.gov/modules/showdocument.aspx?documentid=18282>>

Many Thanks To:

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Department of Workforce Services, Meg Ryan, Utah
Division of Housing and Community Development,
and Kyle Slaughter



RURAL
PLANNING GROUP

Rev Feb 2016

IMPROVEMENT PLANS FOR DOWNTOWN IMPROVEMENTS PHASE 1

**VILLAGE OF HAMPSHIRE
KANE COUNTY, ILLINOIS**

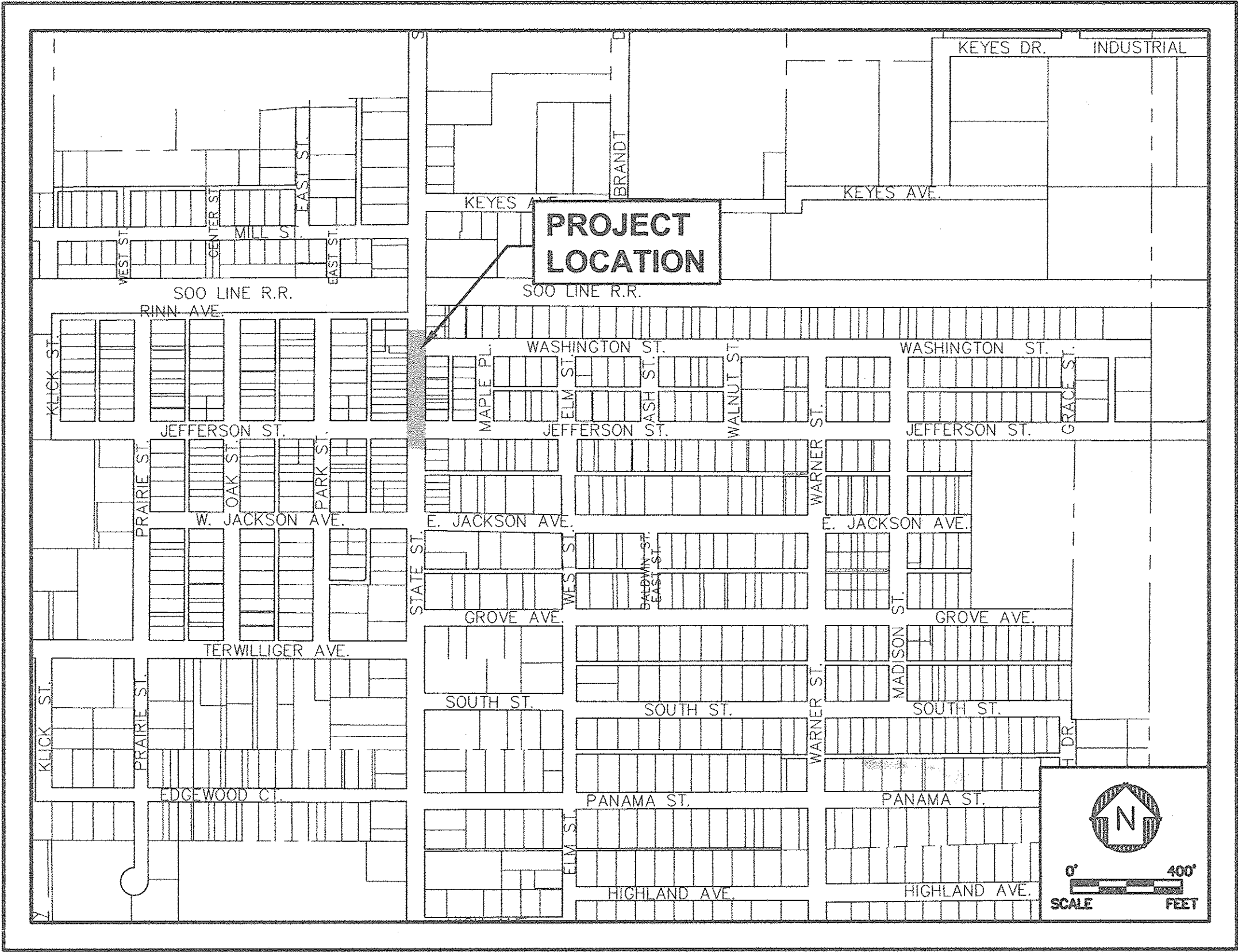
JUNE 2003

VILLAGE OF HAMPSHIRE OFFICIALS

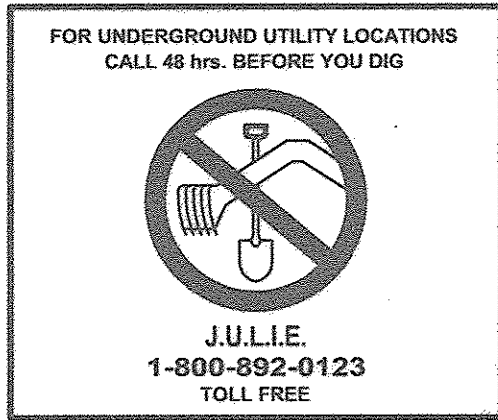
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|--------------------|--------------------------------|
| WILLIAM P. SCHMIDT | PRESIDENT |
| CHUCK ANDERSON | TRUSTEE |
| TOM BROWN | TRUSTEE |
| ED SZYDLOWSKI | TRUSTEE |
| ORRIS RUTH | TRUSTEE |
| WILLIAM SWALWELL | TRUSTEE |
| JAMES TAYLOR | TRUSTEE |
| LINDA R. VASQUEZ | VILLAGE CLERK |
| KATHRYN T. MICHAEL | TREASURER |
| MARK SCHUSTER | VILLAGE ATTORNEY |
| JOHN T. BIDINGER | SUPERINTENDENT OF PUBLIC WORKS |

INDEX OF SHEETS

1. COVER SHEET
2. CONSTRUCTION SPECIFICATIONS AND SUMMARY OF QUANTITIES
3. ELECTRICAL SPECIFICATIONS
4. STATE STREET PLAN AND PROFILE STA. 33+50 TO STA 38+25
5. LIGHTING PLAN
6. ELECTRICAL DETAILS
7. PAVEMENT MARKING PLAN
8. LANDSCAPE PLAN
9. CROSS SECTIONS
10. TYPICAL DETAILS
11. TYPICAL DETAILS
12. TYPICAL DETAILS



LOCATION MAP



BENCHMARKS

1. TOP OF FIRE HYDRANT AT THE SOUTHEAST CORNER OF STATE AND JEFFERSON STREETS U.S.G.S. ELEVATION = 902.69
2. TOP OF FIRE HYDRANT AT THE SOUTHEAST CORNER OF JEFFERSON AND ELM STREETS U.S.G.S. ELEVATION = 901.73



Engineering Enterprises, Inc.
Consulting Engineers
52 Wheeler Road
Sugar Grove, Illinois 60554
630/466-9350

ENGINEER'S CERTIFICATION

I HEREBY CERTIFY THAT THESE PLANS WERE PREPARED UNDER MY DIRECT SUPERVISION.

DATED AT SUGAR GROVE, ILLINOIS,

THIS _____ DAY OF _____, 2003.

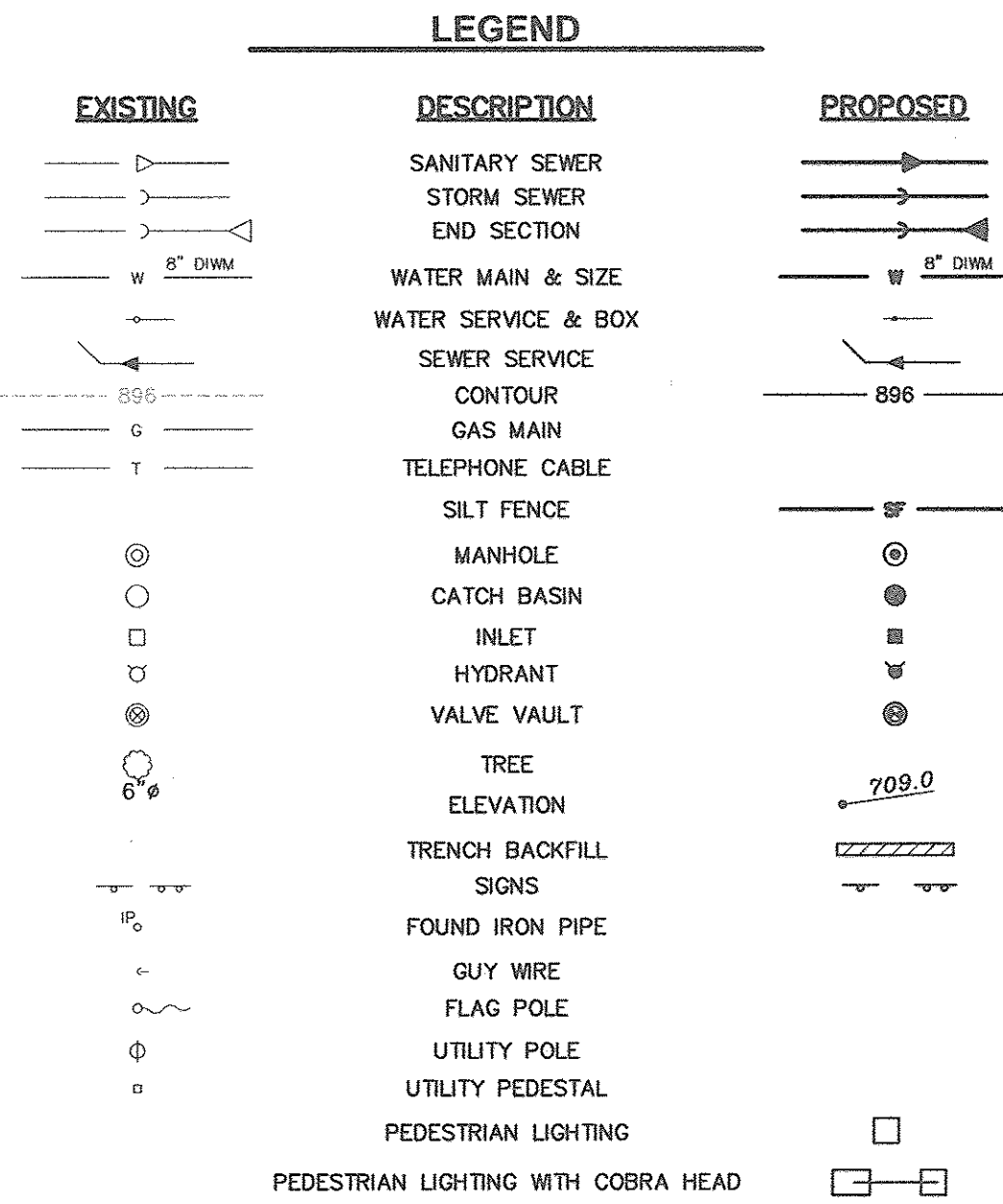
BRADLEY P. SANDERSON, P.E.
ILLINOIS LICENSED PROFESSIONAL ENGINEER NO. 062-052821
EXPIRATION DATE: 11/30/03

REVISIONS		
NO.	DATE	SHEETS

GENERAL NOTES AND CONSTRUCTION SPECIFICATIONS

- THE "STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION" ADOPTED JAN. 1, 2002, THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", FIFTH EDITION, DATED MAY, 1986, AND REVISIONS THERETO, THESE IMPROVEMENT PLANS AND DETAILS SPECIAL PROVISIONS AND CODES AND ORDINANCES OF THE VILLAGE OF HAMPSHIRE, ILLINOIS SHALL GOVERN APPLICABLE PORTIONS OF THIS PROJECT.
 - THE CONTRACTOR SHALL OBTAIN, ERECT, MAINTAIN AND REMOVE ALL SIGNS, BARRICADES, FLAGMEN AND OTHER CONTROL DEVICES AS MAY BE NECESSARY FOR THE PURPOSE OF REGULATING, WARNING OR GUIDING TRAFFIC. PLACEMENT AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH THE APPLICABLE PARTS OF ARTICLE 107.14 OF THE STANDARD SPECIFICATIONS, THE "STANDARD SPECIFICATIONS FOR TRAFFIC CONTROL ITEMS".
 - LOCATIONS OF UTILITIES SHOWN ON PLANS ARE APPROXIMATE ONLY, AND ARE NOT NECESSARILY COMPLETE. CONTRACTOR SHALL MAKE HIS OWN INVESTIGATIONS AS TO LOCATION OF ALL EXISTING UNDERGROUND STRUCTURES, CABLES, UTILITIES AND PIPE LINES.
 - IF EXISTING UTILITY LINES OF ANY NATURE ARE ENCOUNTERED WHICH CONFLICT IN LOCATION WITH NEW CONSTRUCTION, THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND VILLAGE SO THAT THE CONFLICT MAY BE RESOLVED.
 - THE CONTRACTOR SHALL NOTIFY J.U.L.L.E. (1-800-892-0123) AT LEAST TEN DAYS PRIOR TO CONSTRUCTION SO THAT EACH UTILITY COMPANY CAN STAKE OUT ANY UNDERGROUND IMPROVEMENTS THAT THEY MAY HAVE WHICH MIGHT INTERFERE WITH THE PROPOSED CONSTRUCTION.
 - THE CONTRACTOR SHALL BE REQUIRED TO MAKE ARRANGEMENTS FOR THE PROPER BRACING, SHORING AND OTHER REQUIRED PROTECTION OF ALL ROADWAYS, STRUCTURES, POLES, CABLES AND PIPE LINES, BEFORE CONSTRUCTION BEGINS. HE SHALL BE RESPONSIBLE FOR ANY DAMAGE TO THE STREETS OR ROADWAYS AND ASSOCIATED STRUCTURES AND SHALL MAKE REPAIRS AS NECESSARY TO THE SATISFACTION OF THE ENGINEER AND VILLAGE AT HIS OWN EXPENSE.
 - THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ALL PRIVATE AND PUBLIC UTILITIES EVEN THOUGH THEY MAY NOT BE SHOWN ON THE PLANS. ANY UTILITY THAT IS DAMAGED DURING CONSTRUCTION SHALL BE REPAIRED OR REPLACED TO THE SATISFACTION OF THE ENGINEER AND VILLAGE BY THE CONTRACTOR AT HIS OWN EXPENSE.
 - THE CONTRACTOR SHALL EXAMINE THE PLANS AND SPECIFICATIONS, VISIT THE SITE OF THE WORK AND INFORM HIMSELF/HERSELF FULLY WITH THE WORK INVOLVED, GENERAL AND LOCAL CONDITIONS, ALL FEDERAL, STATE AND LOCAL LAWS, ORDINANCES, RULES AND REGULATIONS AND ALL OTHER PERTINENT ITEMS WHICH MAY AFFECT THE COST AND TIME OF COMPLETION OF THIS PROJECT BEFORE SUBMITTING A PROPOSAL.
 - ALL WORK AND MATERIALS SHALL BE IN ACCORDANCE WITH CODE REQUIREMENTS.
 - PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR SHALL CALL THE ATTENTION OF THE ENGINEER TO ANY MATERIAL OR EQUIPMENT HE DEEMS INADEQUATE AND TO ANY ITEM OF WORK OMITTED.
 - THE PAY ITEMS SHALL BE AS NOTED IN THE SUMMARY OF QUANTITIES/PROPOSAL. ANY ITEM OF WORK THAT IS SHOWN ON THE PLANS TO BE PERFORMED BY THE CONTRACTOR, FOR WHICH THERE IS NO PAY ITEM, SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE PROJECT.
 - ALL FINAL ADJUSTMENTS OF CASTING WILL BE ACCOMPLISHED BY THE USE OF PRECAST CONCRETE ADJUSTING RINGS SET IN BUTYL ROPE JOINT SEALANT; MORTAR JOINTS WILL NOT BE ALLOWED. TOTAL HEIGHT OF ADJUSTING RINGS USED SHALL NOT EXCEED EIGHT (8") INCHES. COST FOR ADJUSTMENT IS CONSIDERED INCIDENTAL.
 - THE UNDERGROUND CONTRACTOR SHALL BE RESPONSIBLE TO PLACE ON GRADE AND COORDINATE WITH OTHER CONTRACTORS ALL UNDERGROUND STRUCTURE FRAMES SUCH AS CATCH BASINS, INLETS, MANHOLES, HYDRANTS, BUFFALO BOXES, VALVES, ETC. NO ADDITIONAL COMPENSATION SHALL BE PAID AND SAID ADJUSTMENTS SHALL BE CONSIDERED INCIDENTAL TO OTHER ITEMS OF CONSTRUCTION.
 - THE CONTRACTOR SHALL RESTORE ANY AREA DISTURBED TO A CONDITION EQUAL TO OR BETTER THAN ITS ORIGINAL USE. THIS SHALL INCLUDE FINISH GRADING, ESTABLISHMENT OF A VEGETATIVE COVER (SEEDING OR SOD), GENERAL CLEANUP AND PAVEMENT REPLACEMENT.
 - ALL TRENCHES CAUSED BY THE CONSTRUCTION OF SEWERS, WATER MAINS, WATER SERVICE PIPES AND THE EXCAVATION AROUND CATCH BASINS, MANHOLES, INLETS AND OTHER APPURTENANCES WHICH OCCUR WITHIN THE LIMITS OF EXISTING OR PROPOSED PAVEMENTS, SIDEWALKS AND CURB AND GUTTERS OR WHERE THE EDGE OF THE TRENCH SHALL BE WITHIN TWO (2) FEET OF SAID IMPROVEMENTS SHALL BE BACKFILLED WITH COMPACTED GRANULAR TRENCH BACKFILL OR WITH APPROVED SUITABLE SELECT MATERIAL AND PROPERLY COMPACTED IN ACCORDANCE WITH DIVISION II SECTION 20 OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", FIFTH EDITION, DATED MAY, 1986, AND REVISIONS THERETO.
- TRENCH BACKFILL QUANTITIES FOR PUBLIC UTILITIES AND UTILITY CONDUITS HAVE BEEN COMPUTED AND SHALL BE PAID FOR BASED ON THE FOLLOWING MAXIMUM TRENCH WIDTHS IN ACCORDANCE WITH DIVISION V OF THE "STANDARD SPECIFICATIONS FOR WATER AND SEWER MAIN CONSTRUCTION IN ILLINOIS", FIFTH EDITION, DATED MAY, 1986, AND REVISIONS THERETO. THE DEPTH OF BACKFILL SHALL BE MEASURED FROM THE TOP OF PIPE EMBEDMENT TO THE FINISHED SUBGRADE OR AS NOTED ON THE PLANS.
- | PIPE DIA. | MAXIMUM TRENCH WIDTH | PIPE DIA. | MAXIMUM TRENCH WIDTH |
|-----------|----------------------|-----------|----------------------|
| 6" | 3.58' | 21" | 5.04' |
| 8" | 3.78' | 24" | 5.33' |
| 10" | 3.97' | 27" | 5.63' |
| 12" | 4.17' | 28" | 5.72' |
| 14" | 4.36' | 30" | 5.92' |
| 15" | 4.46' | 33" | 6.21' |
| 16" | 4.56' | 36" | 6.50' |
| 18" | 4.75' | 42" | 7.08' |
| 20" | 4.94' | 48" | 7.67' |
- TRENCH BACKFILL REQUIRED IN EXCESS OF THE QUANTITY BEYOND THE MAXIMUM TRENCH WIDTH SHALL BE CONSIDERED INCIDENTAL TO THE CONTRACT UNLESS AUTHORIZED BY THE ENGINEER.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROVIDING SAFE AND HEALTHFUL WORKING CONDITIONS THROUGHOUT THE CONSTRUCTION OF THE PROPOSED IMPROVEMENTS.
 - THE ENGINEER WILL BE GIVEN FORTY-EIGHT (48) HOURS NOTICE FOR ANY STAKING THAT IS TO BE DONE. EACH OF THE VARIOUS ITEMS OF WORK COVERED BY THIS CONTRACT WILL BE STAKED ONCE. ADDITIONAL STAKING REQUIRED DUE TO THE NEGLIGENCE OF THE CONTRACTOR SHALL BE PAID FOR BY THE CONTRACTOR AT THE CURRENT HOURLY RATE.
 - THE CONTRACTOR SHALL INFORM THE VILLAGE ENGINEER BEFORE WORK COMMENCES ON EACH CATEGORY OF CONSTRUCTION, I.E. WATER MAIN, GRADING, PAVEMENT AND DRAINAGE IMPROVEMENT. A TWENTY-FOUR (24) HOUR NOTICE SHALL BE GIVEN FOR ANY ITEM THAT REQUIRES FINAL TESTING AND INSPECTION SUCH AS WATER MAINS OR SANITARY SEWERS.
 - THE ENGINEER WILL FURNISH THE CONTRACTOR WITH LINES AND GRADES NECESSARY TO THE PROPER PROSECUTION AND CONTROL OF THE WORK. THE CONTRACTOR SHALL CALL THE ATTENTION OF THE ENGINEER TO ANY ERRORS OR DISCREPANCIES WHICH MAY BE SUSPECTED IN LINES AND GRADES WHICH ARE ESTABLISHED BY THE ENGINEER, AND SHALL NOT PROCEED WITH THE WORK UNTIL ANY LINES AND GRADES WHICH ARE BELIEVED TO BE IN ERROR HAVE BEEN VERIFIED OR CORRECTED BY THE ENGINEER OR HIS REPRESENTATIVE.
 - ALL LOT IRONS DAMAGED OR REMOVED DURING CONSTRUCTION OF THIS PROJECT SHALL BE REPLACED BY THE ENGINEER AND SAID COST OF REPLACEMENT SHALL BE PAID BY THE CONTRACTOR.

- BEFORE ACCEPTANCE BY THE VILLAGE AND FINAL PAYMENT, ALL WORK SHALL BE INSPECTED AND APPROVED BY THE VILLAGE ENGINEER. FINAL PAYMENT SHALL BE MADE AFTER ALL OF THE CONTRACTOR'S WORK HAS BEEN APPROVED AND ACCEPTED.
- THE CONTRACTOR WILL HAVE IN HIS POSSESSION ON THE JOB SITE A COPY OF THE PLANS AND SPECIFICATIONS DURING CONSTRUCTION.
- IF ANY APPROVED EQUAL ITEMS ARE REQUIRED, THE CONTRACTOR SHALL CONTACT THE ENGINEER FOR APPROVAL.
- ANY DRAIN AND/OR FIELD TILE ENCOUNTERED BY THE CONTRACTOR DURING THE INSTALLATION OF THE IMPROVEMENTS SHALL BE RETURNED TO ORIGINAL CONDITION. THIS WORK TO BE CONSIDERED INCIDENTAL TO THE CONTRACT.
- ALL ROAD SIGNS, STREET SIGNS AND TRAFFIC SIGNS WHICH NEED TO BE RELOCATED OR MOVED DUE TO CONSTRUCTION SHALL BE TAKEN DOWN AND STORED BY THE CONTRACTOR AT HIS OWN EXPENSE, EXCEPT THOSE WHICH ARE NECESSARY FOR PROPER TRAFFIC CONTROL WHICH SHALL BE TEMPORARILY RESET UNTIL COMPLETION OF CONSTRUCTION OPERATIONS. AFTER COMPLETION OF THE WORK, THE CONTRACTOR SHALL RESET, AT HIS EXPENSE, ALL SAID SIGNS.
- THE CONTRACTOR SHALL DISPOSE OF ALL EXCESS EXCAVATION, UNSUITABLE AND UNUSABLE MATERIALS OFFSITE AND AT AN APPROVED LOCATION IN A MANNER THAT PUBLIC OR PRIVATE PROPERTY WILL NOT BE DAMAGED OR ENDANGERED. THIS WORK IS CONSIDERED AS INCIDENTAL TO THE COST OF THE PROJECT.
- NO EXCAVATIONS WILL BE PERMITTED TO REMAIN OPEN OVER ANY WEEKEND.
- "BAND-SEAL" OR SIMILAR COUPLINGS SHALL BE USED WHEN JOINING SEWER PIPES OF DISSIMILAR MATERIALS.
- THE CONTRACTOR IS RESPONSIBLE FOR COORDINATING ANY REQUIRED INSPECTIONS WITH THE VILLAGE OF HAMPSHIRE.
- SPECIAL ATTENTION IS DRAWN TO THE FACT THAT ARTICLE 105.06 OF THE STANDARD SPECIFICATIONS REQUIRES THE CONTRACTOR TO HAVE A COMPETENT SUPERINTENDENT ON THE PROJECT SITE AT ALL TIMES, IRRESPECTIVE OF THE AMOUNT OF WORK SUBLET. THE SUPERINTENDENT SHALL BE CAPABLE OF READING AND UNDERSTANDING THE PLANS AND SPECIFICATIONS, SHALL HAVE FULL AUTHORITY TO EXECUTE ORDERS TO EXPEDITE THE PROJECT, SHALL BE RESPONSIBLE FOR SCHEDULING AND HAVE CONTROL OF ALL WORK AS THE AGENT OF THE CONTRACTOR. FAILURE TO COMPLY WITH THIS PROVISION WILL RESULT IN A SUSPENSION OF WORK AS PROVIDED IN ARTICLE 108.07.
- THE ENGINEER AND VILLAGE ARE NOT RESPONSIBLE FOR THE CONSTRUCTION MEANS, METHODS, TECHNIQUES, SEQUENCES OR PROCEDURES, TIME OF PERFORMANCE, PROGRAMS OR FOR ANY SAFETY PRECAUTIONS USED BY THE CONTRACTOR. THE CONTRACTOR IS SOLELY RESPONSIBLE FOR EXECUTION OF HIS WORK IN ACCORDANCE WITH THE CONTRACT DOCUMENTS AND SPECIFICATIONS.



SUMMARY OF QUANTITIES

DIVISION A			
ITEM NO.	ITEM	UNIT	QUANTITY
1	TREE REMOVAL (6 TO 15 UNITS DIAMETER)	UNITS DIA.	10
2	LANDSCAPING	LUMP SUM	1
3	TRENCH BACKFILL	CU. YD.	13
4	TOPSOIL, FURNISH AND PLACE, 6"	SQ. YD.	150
5	NITROGEN FERTILIZER NUTRIENT	POUNDS	7
6	PHOSPHORUS FERTILIZER NUTRIENT	POUNDS	7
7	POTASSIUM FERTILIZER NUTRIENT	POUNDS	7
8	PORTLAND CEMENT CONCRETE SIDEWALK, 5"	SQ. FT.	4,942
9	PAVERS	SQ. FT.	120
10	PAVEMENT REMOVAL	SQ. YD.	815
11	SIDEWALK REMOVAL	SQ. FT.	4,942
12	COMBINATION CURB AND GUTTER REMOVAL	FOOT	10
13	PAVEMENT REPLACEMENT (SPECIAL)	SQ. YD.	502
14	STORM SEWERS, CLASS A, TYPE 2, RCP 10"	FOOT	6
15	STORM SEWERS, CLASS A, TYPE 2, RCP 12"	FOOT	7
16	STORM SEWERS, CLASS A, TYPE 2, RCP 18"	FOOT	6
17	STORM SEWER REMOVAL	FOOT	23
18	FIRE HYDRANTS TO BE MOVED	EACH	1
19	FIRE HYDRANTS TO BE REMOVED	EACH	1
20	CATCH BASINS, TYPE A, 4' DIAMETER, TYPE 3 FRAME AND GRATE	EACH	1
21	MANHOLES, TYPE A, 4' DIAMETER, TYPE 1 FRAME, OPEN LID	EACH	2
22	CATCH BASINS TO BE ADJUSTED WITH NEW FRAME AND LID (CLOSED)	EACH	1
23	MANHOLES TO BE ADJUSTED	EACH	3
24	REMOVING CATCH BASINS	EACH	1
25	COMBINATION CONCRETE CURB AND GUTTER, TYPE B-6.12	FOOT	593
26	PAINT PAVEMENT MARKING - LINE, 4"	FOOT	645
27	PAINT PAVEMENT MARKING - LETTERS AND SYMBOLS	SQ. FT.	9
28	PAINT PAVEMENT MARKING REMOVAL	SQ. FT.	90
29	RELOCATE SIGN PANEL	EACH	7
30	REMOVE SIGN PANEL ASSEMBLY	EACH	7
31	REMOVAL OF EXISTING LIGHTING UNIT, SALVAGE	EACH	3
32	BOLLARDS TO BE REMOVED	EACH	2
33	B-BOX TO BE ADJUSTED	EACH	3
34	INSTALL 4 DIAL CLOCK	EACH	1
35	TRENCH AND BACKFILL FOR ELECTRICAL WORK	FOOT	1,320
36	CONDUIT IN TRENCH, RIGID GALVANIZED STEEL, 1 1/2"	FOOT	960
37	CONDUIT IN TRENCH, RIGID GALVANIZED STEEL, 2"	FOOT	240
38	CONDUIT IN TRENCH, RIGID GALVANIZED STEEL, 3"	FOOT	120
39	CONDUIT DIRECTIONAL BORE, RIGID GALVANIZED STEEL, 1 1/2"	FOOT	55
40	CONDUIT DIRECTIONAL BORE, RIGID GALVANIZED STEEL, 2"	FOOT	55
41	WIRING	LUMP SUM	1
42	HANDHOLES, 21 1/2"	EACH	5
43	LIGHTING CONTROLLER WITH METER	EACH	1
44	TRAFFIC CONTROL AND PROTECTION	LUMP SUM	1
45	LIGHT POLE, ALUMINUM, 14'	EACH	6
46	LIGHT POLE, ALUMINUM, 30'	EACH	3



ELECTRICAL SPECIFICATION

GENERAL

- A. General Conditions and Section I Supplementary General Conditions form a part of this Specification.
B. The work covered under this Specification is intended to include the installation of equipment, materials and labor necessary to or reasonably incidental to completing the operating installation of the system of electric wiring and as indicated on the drawings and herein specified.
C. Bidders shall thoroughly examine and be familiar with drawings, specifications and other contract documents, and shall visit the site and determine all existing local conditions affecting work in his contract.

RULES AND REGULATIONS

- A. All work shall be installed in strict accordance with all applicable rules and regulations of local, Village, State and Federal Governments, or other authorities having lawful jurisdiction.
B. All electrical work, materials and equipment shall conform to the requirements of the Local Electric Code, the latest National Electric Code, Electric Utility Co. and Telephone Company.
C. The Contractor shall cooperate with and assist other Contractors on the job in conformity with all trade jurisdictional ruling.

PERMITS AND INSPECTIONS

- A. The Contractor shall make application for all permits and certificates of inspection required by City, Village, Local, State or Federal governments. Permit fee to be paid by Electrical Contractor only if directed by Local Authorities.

DEFINITIONS

- A. Provide means furnish and install.

SCOPE OF WORK

- A. Provide a complete and operating electrical system including all necessary labor, material, equipment, tools, insurance and erection equipment.
B. The work includes, but is not limited to, furnishing and/or installing the following:
1. Distribution Wiring
2. Exterior Lighting
3. Lighting Contractors
4. Lighting Controller
5. Lighting Fixtures and Lamps
6. Metering
7. Outlets and Switches
8. Permits, Fees and Taxes
9. Sound System
C. Where disagreements occur between Plans and Specifications or within either document itself, the item or arrangement of better quality, greater quantity, or higher cost shall be included in the base bid.
D. Contractor may be required to remove from the premises material or work not in accordance with Plans and Specifications and correct unsatisfactory work or material, expense of doing so to be borne by the Contractor.
E. The Contract Drawings contain information to great detail which is considered to be both consistent with their scales and adequate to accomplish their purpose.

APPROVAL OF MATERIALS

- A. Within 15 days after acceptance of Contractor's bid, he shall submit to Engineer for approval a list of manufacturers of equipment proposed for the work.
B. All equipment and materials shall be as specified in these Specifications and on the drawings.
C. All equipment shall meet the requirements of the National Board of Fire Underwriters and bear the label of the Underwriters Laboratories.
D. The work under the Electrical Sections shall also comply with the latest edition of the applicable standards and codes of the following:
1. ANSI - American National Standards Institute
2. ASTM - American Society for Testing Materials
3. CBM - Certified Ballast Manufacturer
4. EIA - Electronics Industries Association
5. ETL - Electrical Testing Laboratories
6. IEEE - Institute of Electrical & Electronic Engineers
7. IPCEA - Insulated Power Cable Engineers Association
8. NEC - National Electrical Code
9. NEMA - National Electrical Manufacturers Association
10. OSHA - Occupational Safety and Health Administration

SHOP DRAWINGS

- A. After receiving approval on the equipment manufacturers, this Contractor shall submit without delay seven (7) copies each of Drawings or cuts of all equipment for the approval of the Engineer.
B. Such submittals must contain outline dimensions, operating clearance and sufficient engineering data to indicate substantial compliance with the Specifications.
C. Where the equipment furnished differs materially from that indicated on the Contract Drawings, or where the Contractor considers additional detail or Shop Drawings essential to the proper fabrication or installation of equipment, he shall prepare such drawings and submit seven (7) blueprint copies for approval.
D. Approval granted on Shop Drawings is rendered as a service only and shall not be considered as a guarantee of measurements or building conditions; nor shall it be construed as relieving the Contractor of basic responsibilities under the Contract.
E. Before placing any orders for equipment or commencing with any work at the job site, the Electrical Contractor shall have approved Shop Drawings in his files from the Engineer, the Power Company, and Village approval.
F. Shop Drawings shall be submitted for the following:
1. Circuit Breakers
2. Lighting Controller
3. Lighting Fixtures/Poles
4. Outlet Devices
5. Sound System

GUARANTEE

- A. The Contractor shall guarantee all work installed by him or his Subcontractor to be free from defects in material and workmanship for a period of one (1) year following the date of final acceptance of the work, unless a longer period is stipulated under specified headings, and he shall repair or replace, at no additional cost to the Owner, any material or equipment developing defects and shall also make good any damage caused by such defects or the correction of defects. Repairs or replacement shall bear additional 12 month guarantee, as originally called for, dated from the final acceptance of the repair or replacement. This requirement shall be binding even though it will exceed product guarantees normally furnished by some manufacturers.

INTERFERENCES

- A. The Contractor shall confer with other contractors at the site to avoid interference so that a maximum of headroom and clearance may be obtained. In the event that interferences develop, the Engineer's decision will be final and no additional compensation will be allowed for the moving of misplaced wiring, lighting fixtures, or equipment.

SLEEVES

- A. Where conduits pass through concrete floors or walls, galvanized sheet metal sleeves shall be set. Provide waterproofing per Engineer's specifications.

CUTTING AND PATCHING

- A. The Contractor shall do all cutting and patching of building materials required for the installation of work herein specified.

MOUNTING HEIGHTS

- A. Mounting heights of electrical items shall be as listed below, unless otherwise indicated. Dimensions are above finished floor, unless otherwise indicated. In areas where code requires different mounting heights, as in hazardous areas, comply with code requirements.
B. Lighting Controller: 6'-0" to center line of highest handle, or per code.

LIGHTING FIXTURES

- A. The Electrical Contractor shall provide all lighting fixtures as indicated on the drawings, complete with all wiring and connections, and proper labels for fabrication and wiring.
B. Lighting fixtures of the specific make and type mentioned on the drawings shall be included in the Contractor's bid and will form the basis on which the Contract will be awarded.
C. Provide all lamps for fixtures.
D. Provide all fixture supports and accessories for each fixture for ceiling design as installed.
E. Outdoor lighting poles 20 feet in height or greater shall be equipped with vibration dampers.
F. Pole base covers shall be metal and shall match pole color.
G. All lighting fixtures shall be clean before final acceptance.

WIRING DEVICES

- A. Wall Toggle Switches: 120/277 Volts, 20 AMP.
1. Manufacturers:
a. Arrow Hart Model: 1191
b. Hubbell Model: CS2121
c. P & S Model: 20AC1
2. Color: Brown/black.
B. Receptacles: 125 Volts, 20 AMP, NEMA 5-20, commercial grade. Brown or black.
1. Standard Receptacle Manufacturers:
a. Arrow Hart Model: 5932
b. Hubbell Model: CR3962
c. P & S Model: 5932
2. Ground Fault Circuit Interrupter Receptacle Manufacturers:
a. Arrow Hart Model: 6F5842
b. Hubbell Model: 6F5962
c. P & S Model: 2041-5
C. Plate Covers:
1. Materials: Type 302 stainless steel.
2. Flush Mounting Plates: Beveled type with smooth rolled outer edge.
3. Surface Box Plates: Beveled, sloped, pressure formed for smooth edge to fit box.
4. Weatherproof Plates: Cast metal or plastic, gasketed. Provides spring-loaded gasketed doors for receptacles as per NEC.
a. Manufacturer Tay Mac.

LIGHTING CONTACTORS

- A. 120V, 2 pole, 200A mechanically or electrically held lighting contactors with enclosure.

FULL BOXES, JUNCTION BOXES

- A. Provide pull boxes, fittings or junction boxes where indicated or where required to facilitate the pulling or the making of connections. All boxes shall be galvanized and of the same material as specified for outlet boxes in the same location. Pull and junction boxes shall each be provided with removable cover, marked "Electrical". The covers of all boxes shall be accessible.

ELECTRICAL SERVICE

- A. Provide overhead, 120/240V, 1 phase, 3 wires, 60 hertz service from the Utility Company transformer to the lighting controller.
B. Exact location of transformer shall be as directed by the Utility Company.
C. The electric service and metering equipment shall be complete in all details and shall meet all requirements of the Utility Company. Optional facilities charges will be paid by the Owner.

LIGHTING CONTROLLER

- A. Where shown, provide lighting controller with all accessories.
B. The controller shall meet all requirements of local and national codes and requirements of the Utility Company.
C. Controller shall be rated for service entrance use.

EXTERIOR LIGHTING FOUNDATIONS OR BASES

- A. Provide all concrete foundations or bases for exterior lighting fixtures as called for on the drawings.

WIRE AND CABLE

- A. All wire shall be copper with type "THHN/TMVN" insulation rated at 600 Volts and minimum #12 AWG size unless noted otherwise.
B. Phase Identification: Branch circuit conductors shall be color-coded to differentiate the phases, the same color being assigned to the same phase throughout the job. Phase indication shall be as follows or similar as allowed by code:
240/120 Volts
Phase A - Black
Phase B - Red
Neutral - White
Ground - Green

RACENAYS

- A. All conduit shall be minimum of 1/2" size unless noted otherwise. Provide minimum of 3/4" conduit for underground, under floor and exterior installations as well as telephone, data, TV and P.A. systems.
1. Outdoor Locations - Exposed: Galvanized rigid steel (GRS) conduit and water-tight fittings.
2. Outdoor Locations - Underground: Galvanized rigid steel (GRS) conduit and water-tight fittings.

STENCILING AND IDENTIFICATION

- A. The Contractor shall provide engraved nameplates or neatly stencil with paint (color to be determined at time of installation) cabinets, switchgear, boxes, conduit, phases, etc. to clearly indicate the equipment controlled by respective apparatus along with indicated voltages.
B. Stenciling or nameplates shall be on outside of boxes, cases, conduits, etc., except cabinets, which, unless otherwise indicated, shall be stenciled on the side of the door.
C. Stenciling or nameplates shall be done after all general painting has been completed.
D. The following equipment shall be identified: Electrical Service

PAINTING

- A. All painting shall be done by the General contractor unless specified elsewhere.
B. All metal work laid directly underground shall be painted with one heavy coat of Koppers Bimastic No. 50 paint, by the Contractor.
C. Factory finishes damaged shall be restored in field by the Contractor.

ADJUSTMENTS

- A. The Contractor and Subcontractor shall make all adjustments to each system and equipment which he has installed, in accordance with the intent of the Plans and Specifications.

HOUSEKEEPING AND CLEANUP

- A. The Contractor shall expedite and handle their equipment and material so as not nuisance to other trades. Specific approval shall be secured from the Job super on-the-site storage areas and relocation shall be handled promptly when so req.
B. The Contractor shall handle any leftover material or rubbish caused by installatic as not to cause a nuisance to other trades. All such materials shall be removed site as requested by the Job superintendent at this Contractor's expense.
C. The Contractor shall be responsible for cleaning and verifying underground condit all foreign debris. Conduit to be cleaned and swabbed clean.
D. After the completion of the installation, the entire system shall be thoroughly clea: remove all paint, oil, and other foreign material. Clean all foreign paint, grease, c: unrequired labels and stickers, etc., from all fixtures, equipment, etc.

GROUNDING

- A. Provide all ground rods, grounding clamps, jumpers and cable as shown and as r: the Local Code Authorities. Ground rods shall be copper and a minimum of 3/4" in length.
B. Provide continuity of ground throughout the building conduit system.
C. All ground clamps or connectors shall be of a forged or cast type of copper al U.L. listed for grounding use.
D. Grounding electrode system shall comply with NEC.

SUPERVISION

- A. The Contractor shall provide the service of a competent superintendent or fore be thoroughly familiar with all phases of work under his contract or subcontract, c shall have authority to act for his principals.

TESTING

- A. After wires and cables are in place and connected to devices and equipment, th system shall be tested for shorts and grounds.
B. All hot wires, if shorted and grounded, shall be removed and replaced, if trouble is within conduit.
C. A voltage test shall be made at the last outlet on each circuit. If drop in potential is excessive, Contractor will be required to correct the condition by locating partly grounded conductor or high resistance splice.
D. All grounds, shorts and high resistance splices shall be rectified as directed at the operation, except as mentioned before.
E. Any wiring device, electrical apparatus or lighting fixture furnished under this contract, if grounded or shorted on any integral "live" part, shall be removed and the trouble rectified by replacing all defective parts or materials, as directed.
F. Use suitable test instrument to measure resistance to ground of system using the fall-of-potential test. Where tests show resistance to ground of over 25 ohms, ground system shall be augmented by additional electrode(s) of any type specified in NEC Article 250 to reduce resistance below 25 ohms.

AS-BUILTS

- A. The contractor shall mark up a set of prints for "as-builts" showing all changes from the original drawings.

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Engineering Enterprises, Inc.
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52 Wheeler Road
Sugar Grove, Illinois 60554 630/466-9350

VILLAGE OF HAMPSHIRE
KANE COUNTY, ILLINOIS

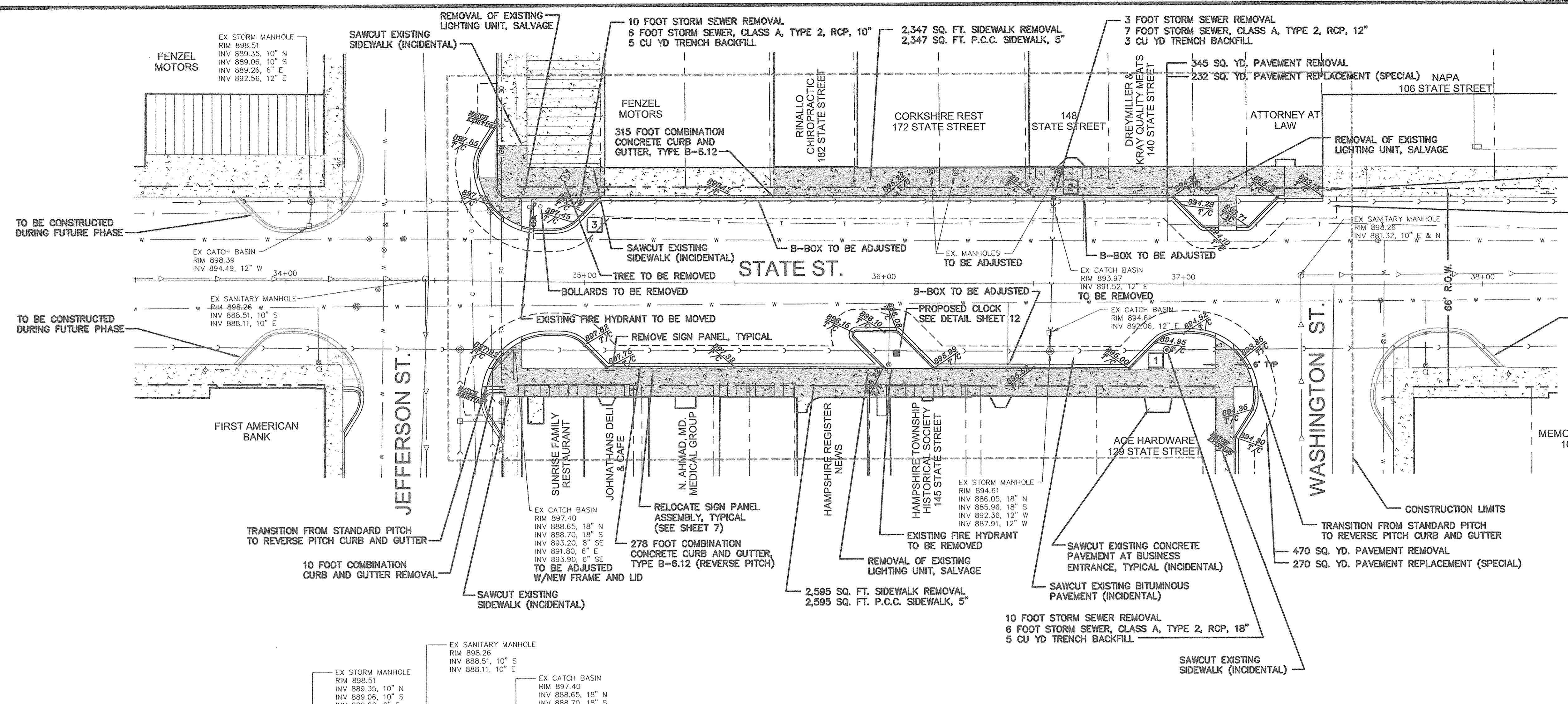
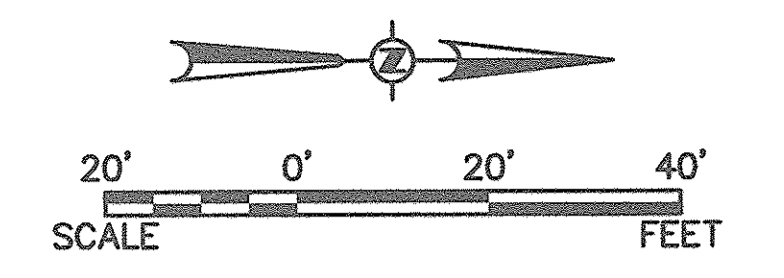
DOWNTOWN IMPROVEMENTS
PHASE 1

ELECTRICAL SPECIFICATIONS

PATH:H:\SDSKPROJ\HA0247\DWG\HA024703

Table with project details: DATE: JUNE 2003, PROJ. NO.: HA0247, FILE NO.: HA024703, SHEET: 3 OF 12

Table with 3 columns: NO., DATE, REVISIONS



SAWCUT EXISTING SIDEWALK (INCIDENTAL)
SAWCUT EXISTING BITUMINOUS PAVEMENT (INCIDENTAL)

TO BE CONSTRUCTED DURING FUTURE PHASE

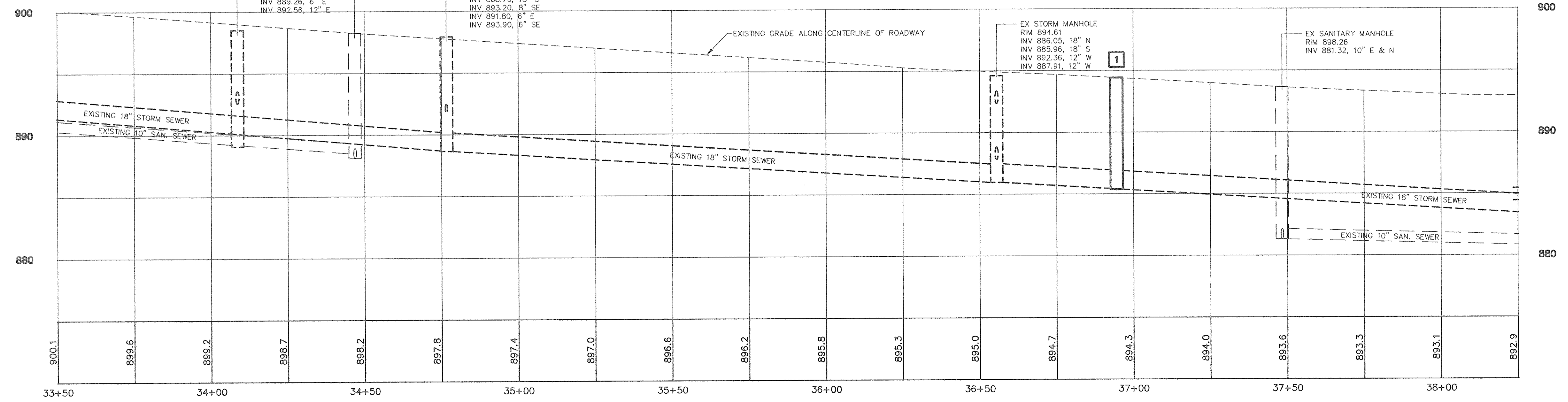
ELLA JOHNSON MEMORIAL PUBLIC LIBRARY
109 STATE STREET

CONSTRUCTION LIMITS
TRANSITION FROM STANDARD PITCH TO REVERSE PITCH CURB AND GUTTER

STORM SEWER SCHEDULE

- 1 MANHOLE
TYPE A, 4' DIA.
TYPE 1 FRAME, OPEN LID
RIM 894.50
INV 885.46, 18" N & S
- 2 CATCH BASIN
TYPE A, 4' DIA.
TYPE 3 FRAME AND GRATE
RIM 894.68 (T/C)
INV 891.83, 12" E
- 3 MANHOLE
TYPE A, 4' DIA.
TYPE 1 FRAME, OPEN LID
RIM 896.95
INV 888.34, 10" N & S

SCALE:
HORZ. 1" = 20'
VERT. 1" = 5'



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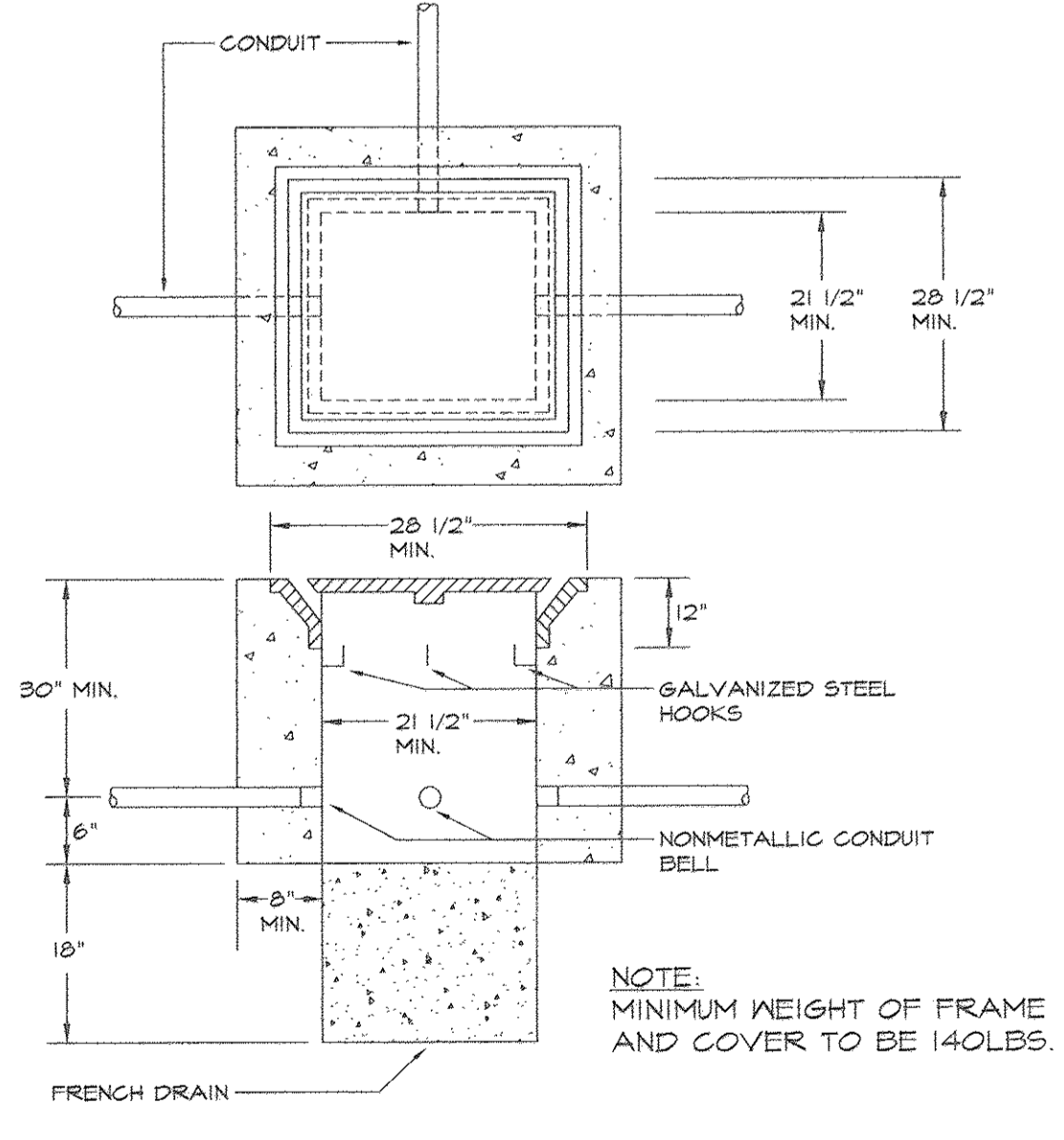
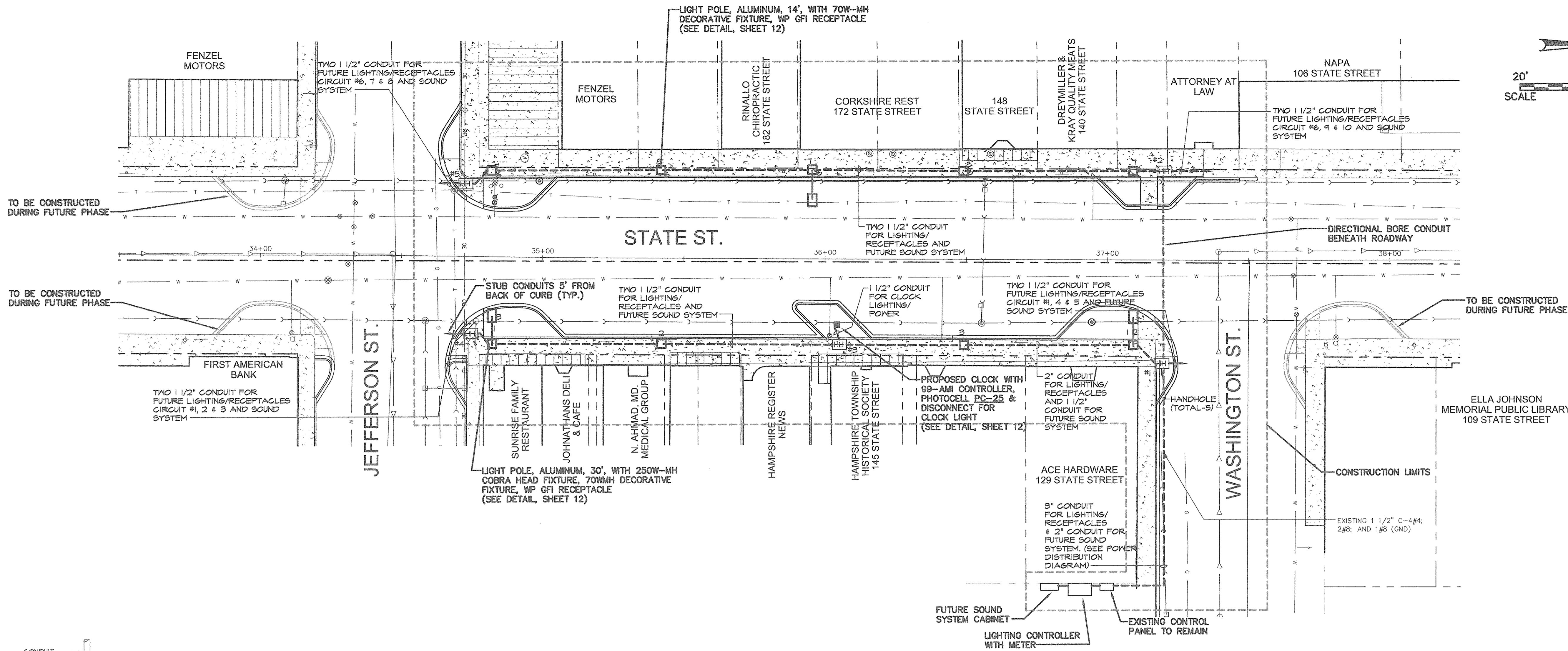
VILLAGE OF HAMPSHIRE
KANE COUNTY, ILLINOIS

NO.	DATE	REVISIONS

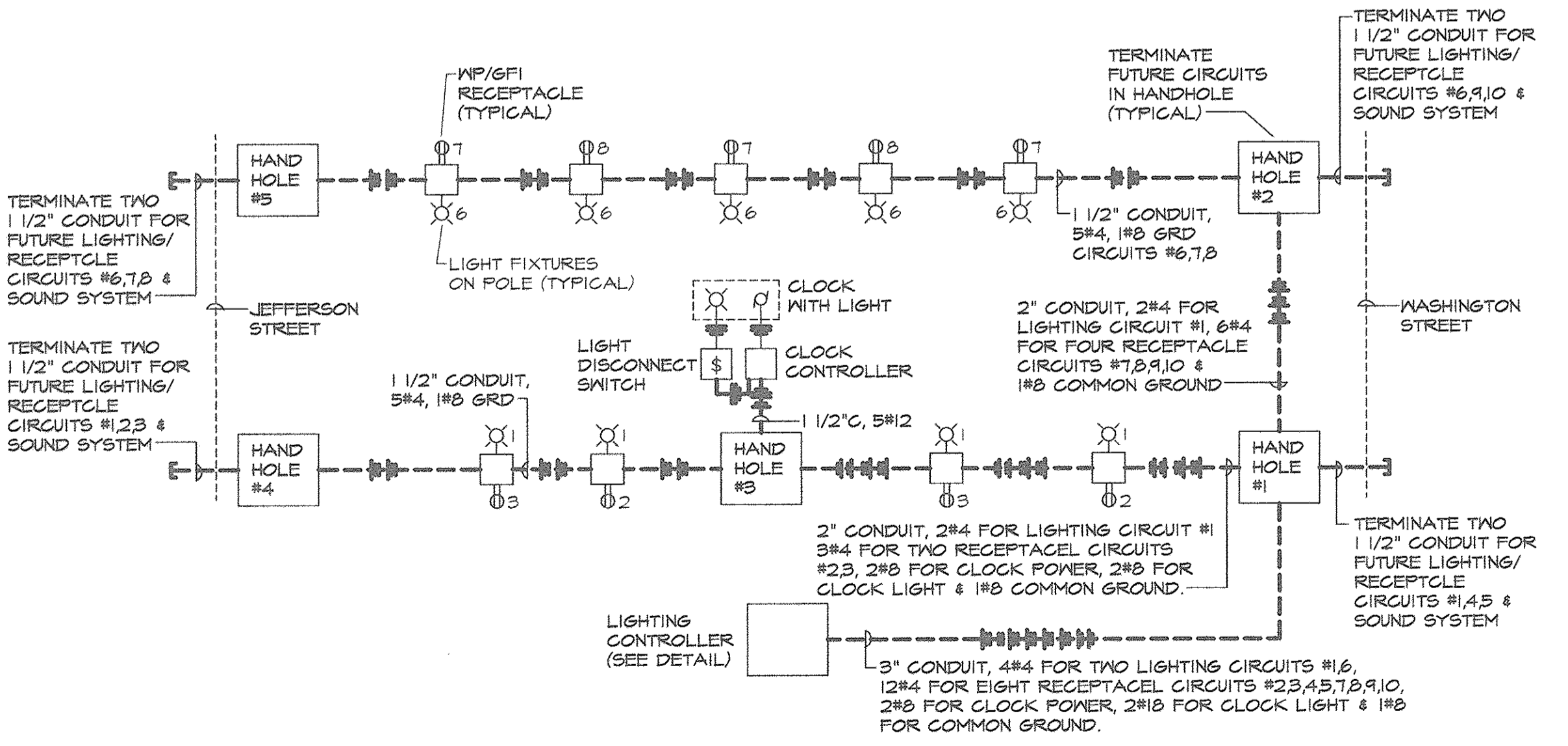
DOWNTOWN IMPROVEMENTS
PHASE 1

STATE STREET
PLAN AND PROFILE
STA 33+50 TO STA 38+25

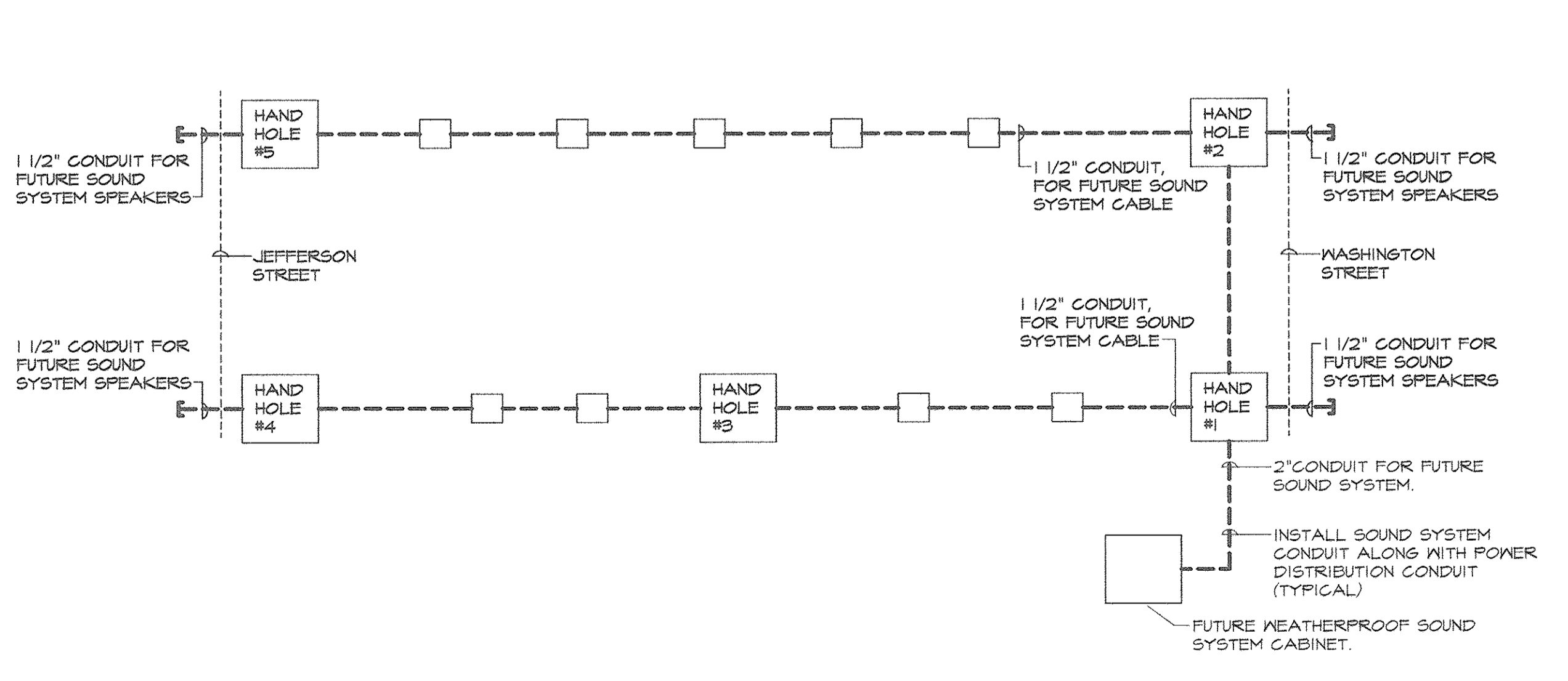
DATE	JUNE 2003
PROJ. NO.	HA0247
FILE NO.	HA024705
SHEET	4 OF 12



HANDHOLE DETAIL (CONCRETE)
N.T.S.



SCHEMATIC DIAGRAM - POWER DISTRIBUTION
N.T.S.



SCHEMATIC DIAGRAM - SOUND SYSTEM
N.T.S.

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NO.	DATE	REVISIONS

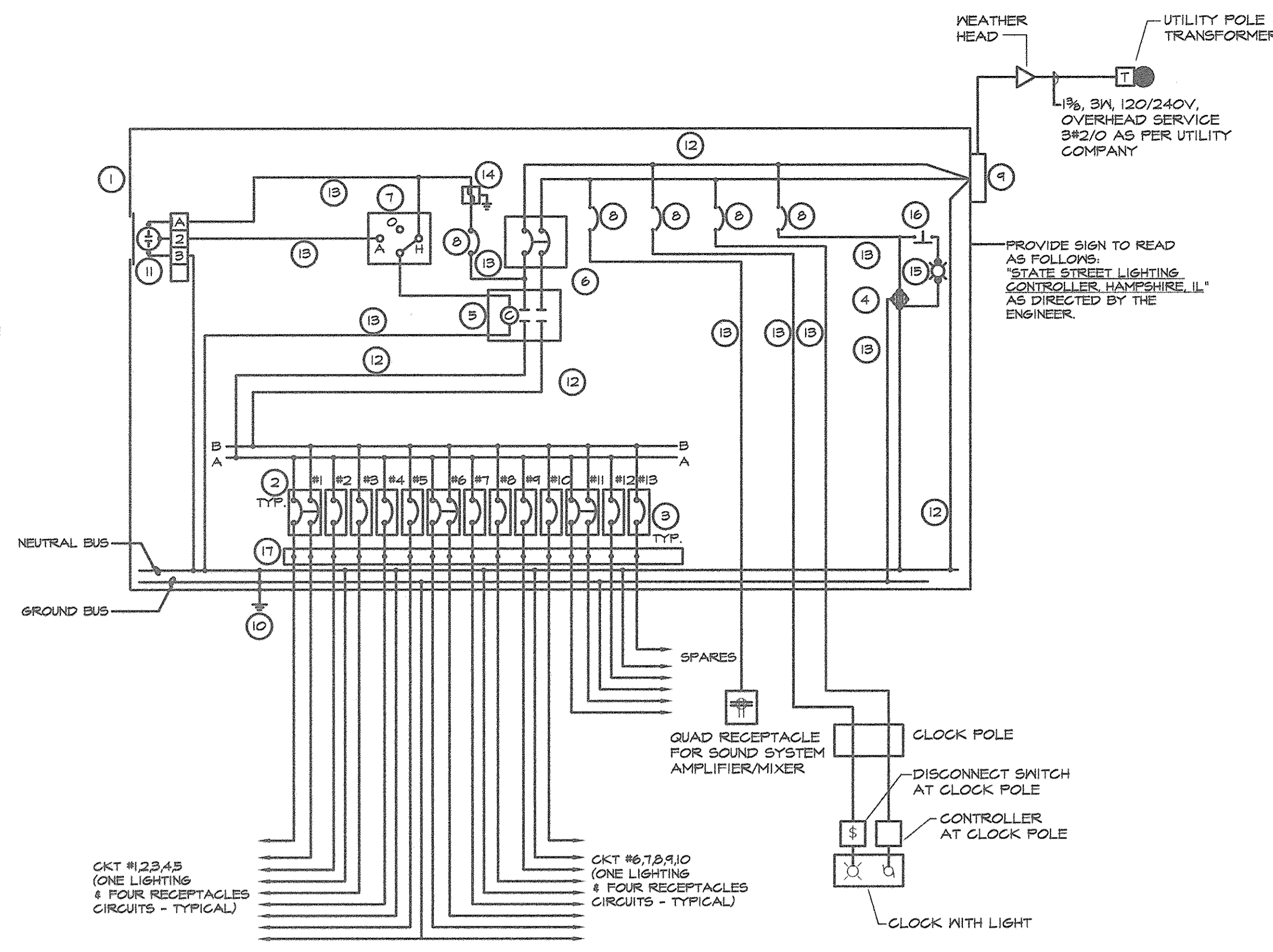
**DOWNTOWN IMPROVEMENTS
PHASE 1**

LIGHTING PLAN

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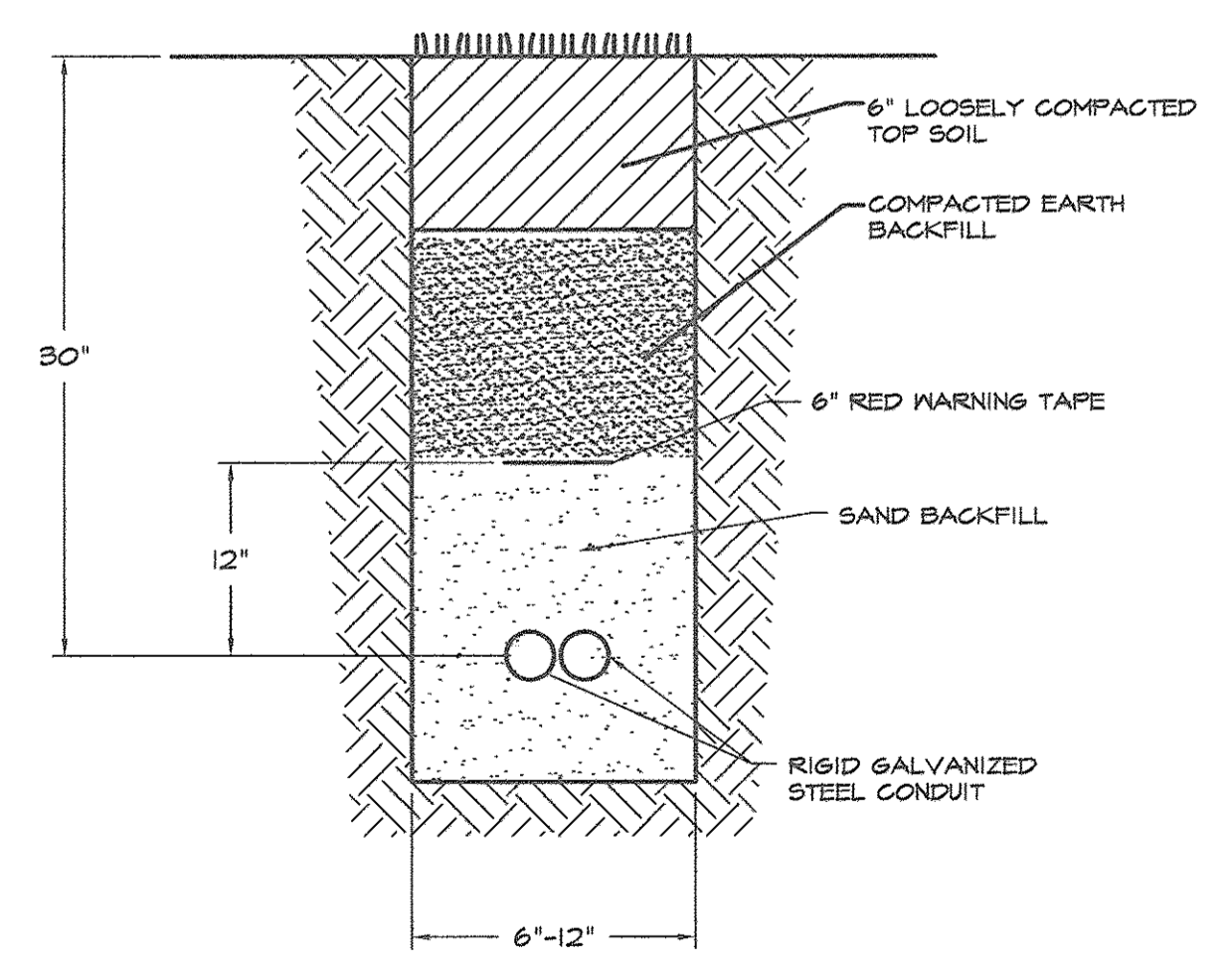
DATE	JUNE 2003
PROJ. NO.	HA0247
FILE NO.	HA024706
SHEET	5 OF 12

- KEYED NOTES:**
- 1 STAINLESS STEEL (ANSI 304) CONTROLLER, SIZE AS REQUIRED. PROVIDE SIGN AS INDICATED ON SCHEMATIC.
 - 2 POLE, 20 AMP, BRANCH CIRCUIT BREAKER (TYPICAL)
 - 3 1 POLE, 20 AMP BRANCH CIRCUIT BREAKER (TYPICAL)
 - 4 20 AMP 6FI DUPLEX RECEPTACLE.
 - 5 2 POLE, 200 AMP CONTACTOR.
 - 6 2 POLE, 200 AMP MAIN CIRCUIT BREAKER.
 - 7 H-O-A THREE POSITION SELECTOR SWITCH.
 - 8 1 POLE, 20 AMP CIRCUIT BREAKER.
 - 9 UTILITY METER SOCKET AND M.P. HOUSING.
 - 10 GROUND ROD, 3/4"x10'-0" COPPERWELD #14 BARE SOL. GR ELECTRODE ALL GROUND CONNECTIONS SHALL BE BY THE EXOTHERMIC WELDING PROCESS.
 - 11 PHOTO ELECTRIC CELL.
 - 12 #8/0 AWG WIRE
 - 13 #12 AWG WIRE
 - 14 SURGE PROTECTOR, EDCO # 4803.
 - 15 LAMP HOLDER, BRYANT #4224 W/WIRE GUARD AND 60W, A19 LAMP
 - 16 DOOR SWITCH 'ON' WHEN DOOR IS OPEN, RATED 20AMPS.
 - 17 TERMINAL BLOCK.



CONTROLLER - SCHEMATIC WIRING DIAGRAM
N.T.S.

- ELECTRICAL NOTES**
- EN-1. ALL WORK SHALL CONFORM TO THE LATEST EDITION OF NATIONAL ELECTRICAL CODE, VILLAGE OF HAMPSHIRE STANDARDS, AND ALL LOCAL GOVERNING CODES. ALL WIRE SHALL BE COPPER WITH TYPE "XLP/USE" INSULATED RATED AT 600 VOLTS, U.L. LISTED.
 - EN-2. REVIEW ALL DRAWINGS AND SPECIFICATIONS. COORDINATE AND PERFORM ELECTRICAL INSTALLATION WITH OTHER TRADE WORK.
 - EN-3. WORK SHOWN INS DIAGRAMMATIC. DO NOT SCALE DRAWINGS. VERIFY ALL DIMENSIONS AND DETAILS IN THE FIELD.
 - EN-4. COORDINATE ELECTRICAL SERVICE INSTALLATION WITH COMMONWEALTH EDISON COMPANY. PROVIDE WATERTIGHT METER SOCKET ON THE CONTROLLER. ENTIRE INSTALLATION SHALL BE DONE IN ACCORDANCE WITH COMMONWEALTH EDISON COMPANY SYSTEM STANDARDS.
 - EN-5. PROVIDE A CONTROLLER CABINET OF ADEQUATE SIZE TO ACCOMMODATE ALL EQUIPMENT AND DEVICES BUT NO LESS THAN THE NOTED DIMENSIONS. PROVIDE ADEQUATE RAIN-TIGHT VENTILATION FOR INTAKE AND EXHAUST WHICH WILL NOT DRAIN WATER INTO THE CABINET.
 - EN-6. PROVIDE A PHOTOCELL IN THE CONTROLLER, VERIFY THE MOUNTING HEIGHT.
 - EN-7. PROVIDE 2 #4 AWG WIRE FOR LIGHTING CIRCUIT AND 3 #4 AWG WIRE FOR POLE RECEPTACLE CIRCUITS, PLUS ONE COMMON #8 AWG GROUND WIRE FOR LIGHTING AND RECEPTACLE CIRCUITS. ALTERNATE RECEPTACLE CIRCUIT NUMBER AT EACH ADJACENT POLE.

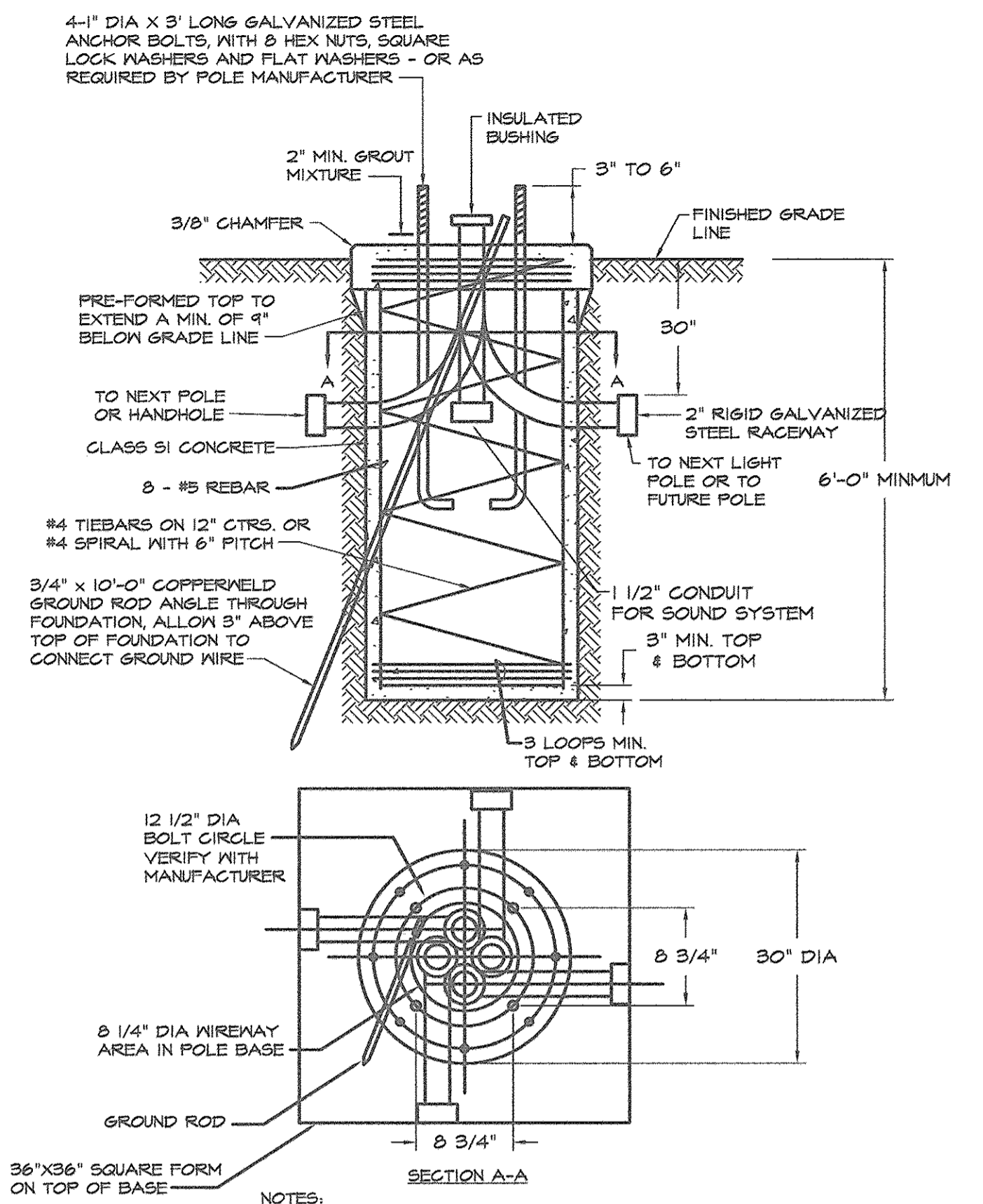


TRENCH SECTION - SOIL AREAS
N.T.S.

FOUNDATION DESIGN TABLE

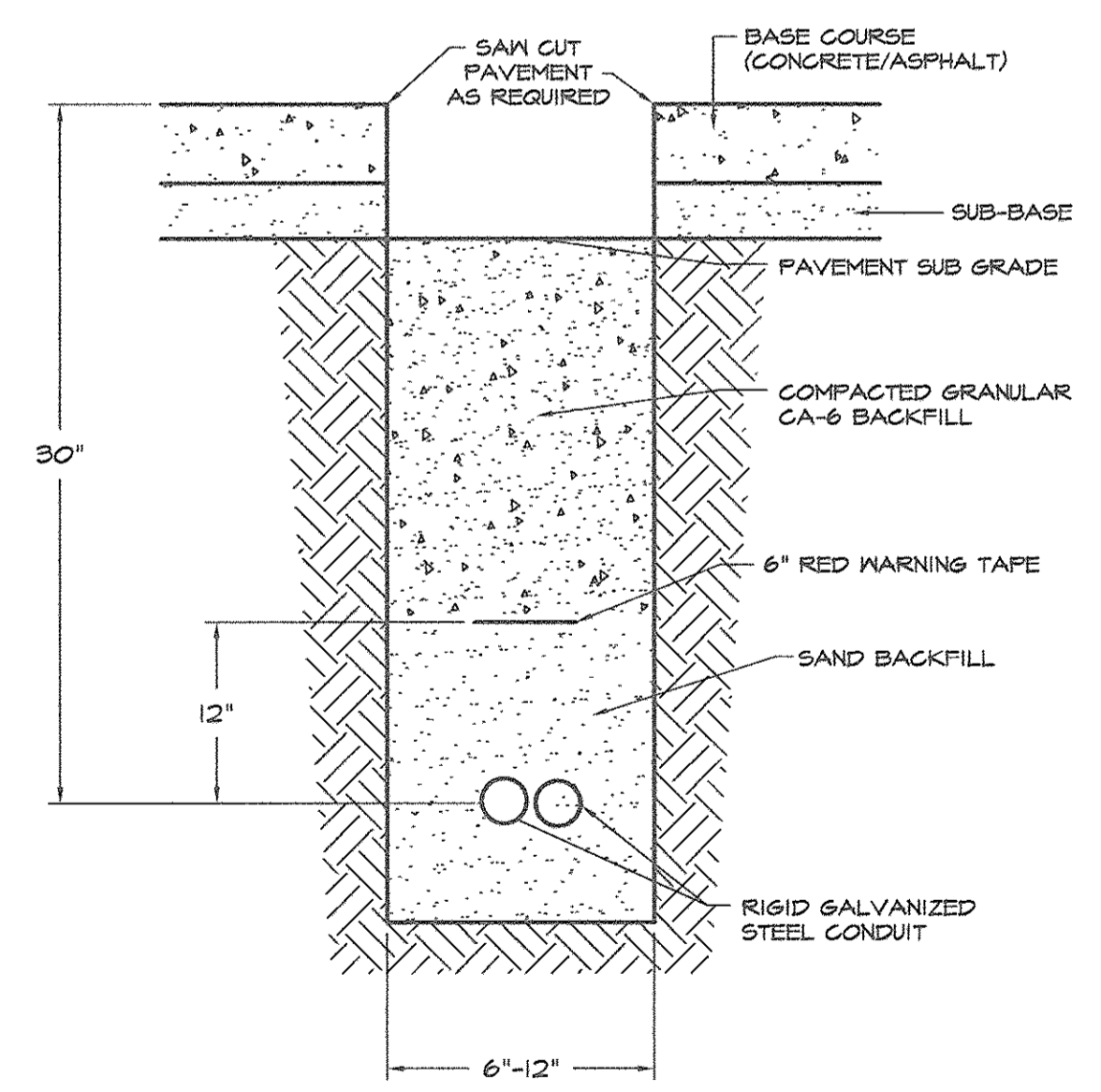
TYPE OF SOIL	DESIGN DEPTH OF FOUNDATION		REINFORCEMENT IN FOUNDATION			
	SINGLE ARM	TWIN ARM	VERT BARS	SPIRAL	VERT BARS	SPIRAL
SOFT CLAY	15'-0"	15'-0"	12'-6"	122'	14'-3"	141'
MEDIUM CLAY	9'-6"	10'-4"	9'-0"	90'	10'-0"	100'
STIFF CLAY	7'-0"	8'-0"	6'-6"	66'	7'-6"	76'
LOOSE SAND	4'-0"	10'-0"	8'-6"	86'	4'-6"	44'
MEDIUM SAND	8'-3"	9'-0"	8'-0"	78'	8'-6"	85'
DENSE SAND	7'-4"	9'-0"	7'-6"	75'	8'-6"	85'
ROCK OR SOLIDIFIED SLAB	5'-0"	5'-0"	NONE	NONE	NONE	NONE

- NOTES:**
1. THE ENGINEER SHALL DETERMINE THE CLASS OF SOIL DURING EXCAVATION AND SELECT THE DESIGN DEPTH OF THE FOUNDATION FROM THE DESIGN TABLE.
 2. EXCAVATION OF THE POLE FOUNDATION SHALL BE MADE WITH AN AUGER 24" IN DIAMETER.
 3. THE ANCHOR BOLTS SHALL BE A TACK WELDED TYPE BOLT OR HOOK TYPE BOLT. COLD BENDING OF THE HOOK BOLT WILL NOT BE ALLOWED.
 4. THE ANCHOR BOLTS AND RACEWAYS SHALL BE PROPERLY SECURED IN PLACE BEFORE THE CONCRETE IS PLACED IN THE FORM.
 5. THE ENTIRE LENGTH OF THE ANCHOR BOLTS AS WELL AS THE NUTS AND WASHERS SHALL BE HOT DIP GALVANIZED IN ACCORDANCE WITH THE REQUIREMENTS OF ASTM DESIGNATION A153.
 6. THE CONTRACTOR SHALL COORDINATE EXTENSION OF THE ANCHOR BOLTS ABOVE THE TOP OF FOUNDATION WITH THE BREAKAWAY DEVICE MANUFACTURER'S REQUIREMENTS. IF THE LIGHT POLE IS MOUNTED WITHOUT THE BREAKAWAY DEVICE, THE ANCHOR BOLTS SHALL PROJECT 3" ABOVE THE TOP OF THE FOUNDATION. THE CONTRACTOR SHALL CONFIRM ANCHOR BOLT EXTENSION WITH THE ENGINEER.
 7. RACEWAYS SHALL PROJECT 1" ABOVE THE TOP OF THE FOUNDATION.
 8. CONCRETE SHALL BE CLASS "51". THE CONCRETE FOUNDATION MUST BE CURED FOR 10 DAYS BEFORE THE LIGHT STANDARD IS ERECTED.
 9. THE TRENCH SHALL BE BACKFILLED AND FIRMLY COMPACTED BEFORE THE LIGHT IS ERECTED.

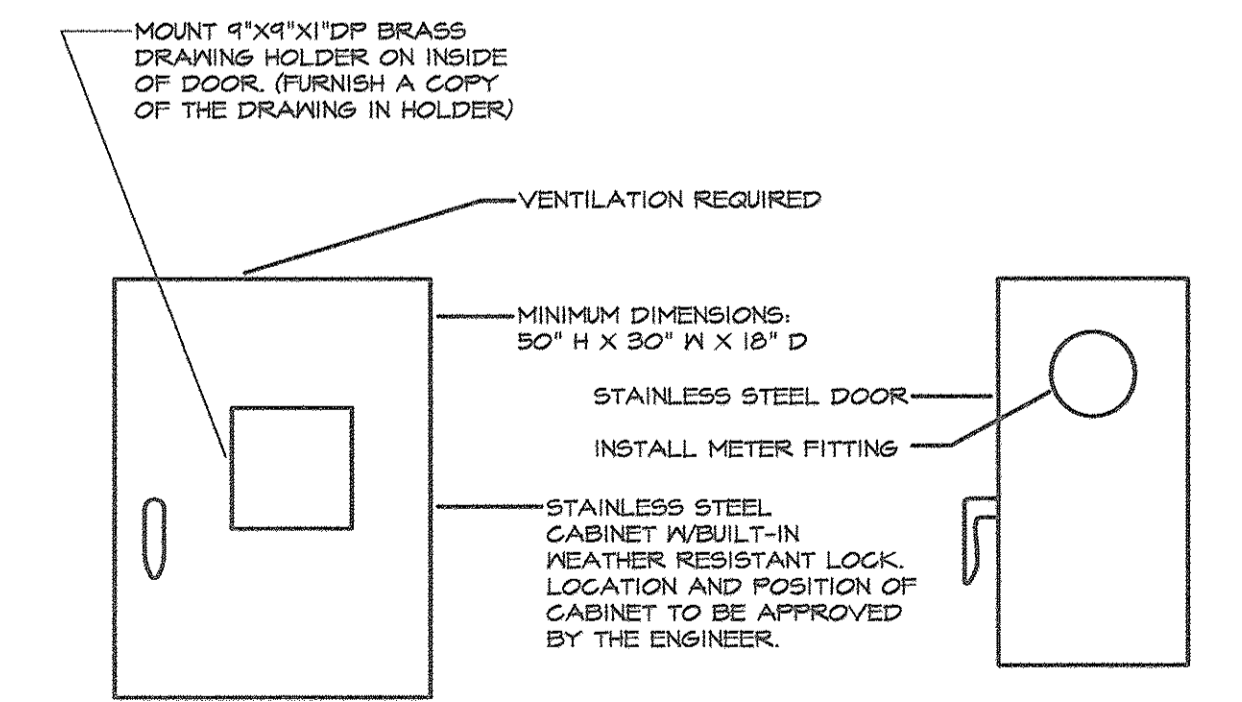


- NOTES:**
- 1) VERIFY ALL POLE RELATED DIMENSIONS AND INFORMATION WITH POLE MANUFACTURER PRIOR TO FABRICATION OF FOUNDATION.
 - 2) CONTRACTOR IS RESPONSIBLE FOR RESTORATION OF DISTURBED AREAS.

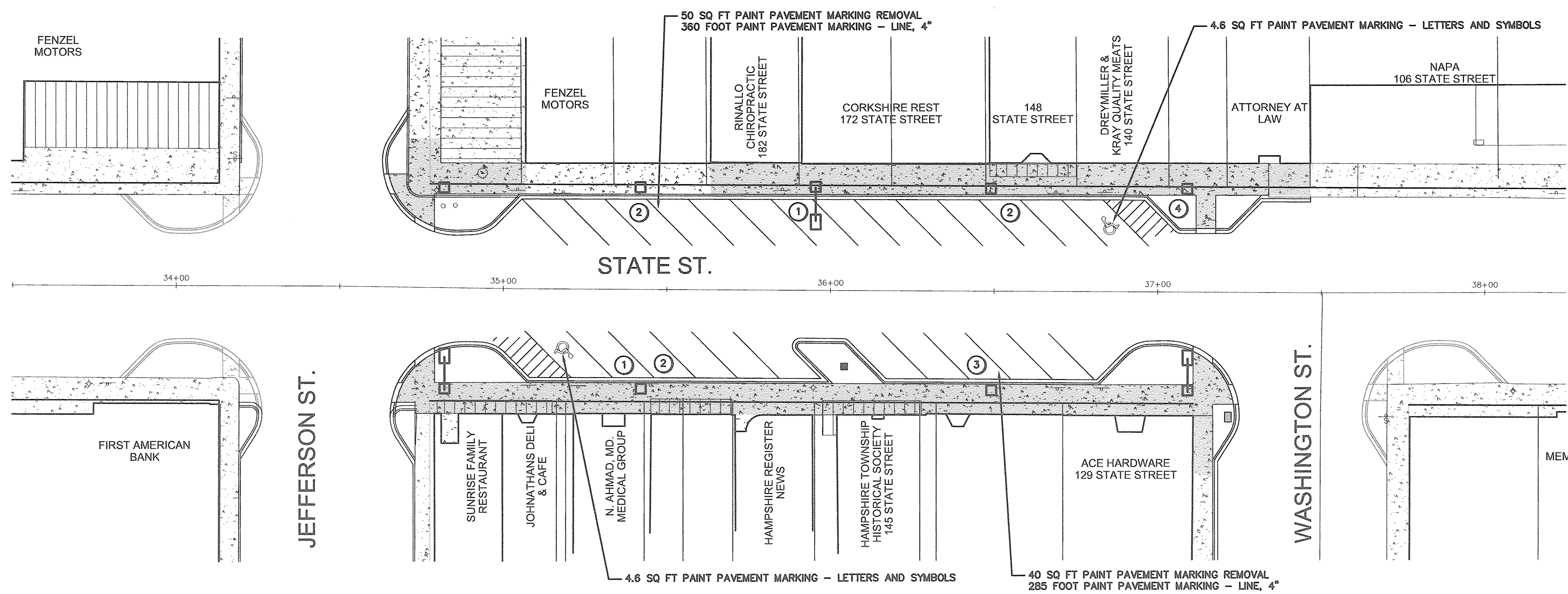
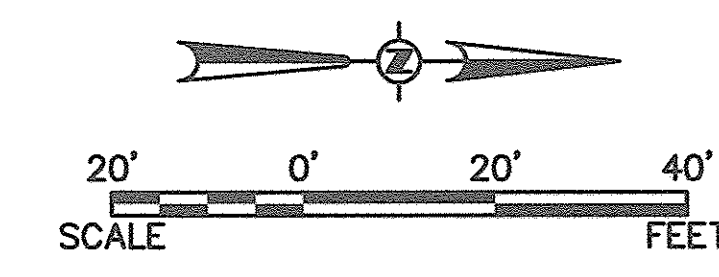
LIGHT POLE FOUNDATION DETAIL
N.T.S.



TRENCH SECTION - PAVED AREAS
N.T.S.



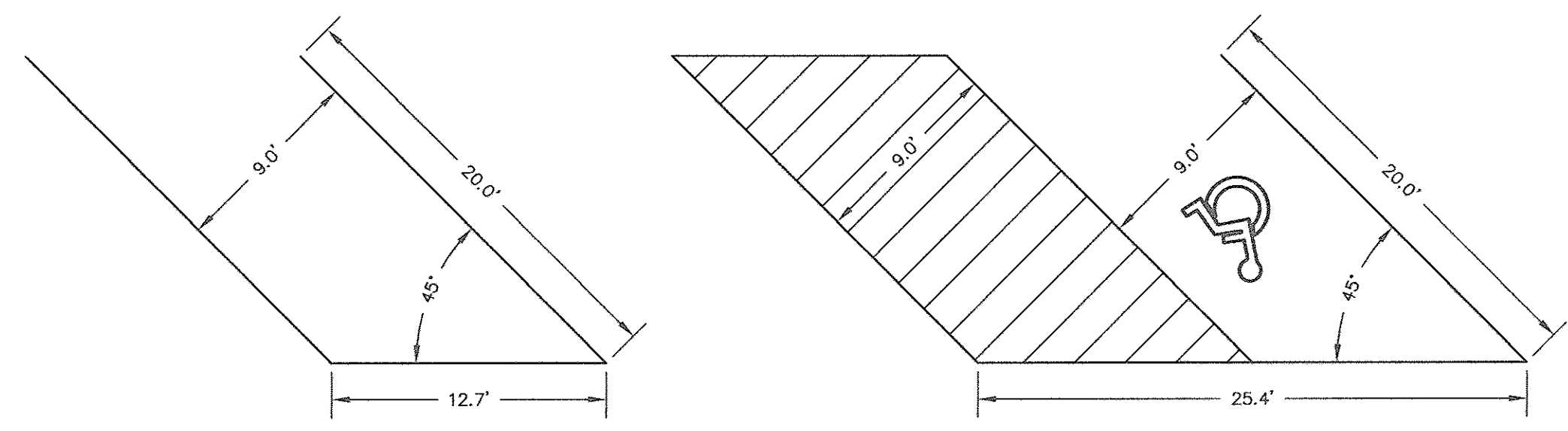
LIGHTING CONTROL CABINET
N.T.S.



SIGN PANEL SCHEDULE

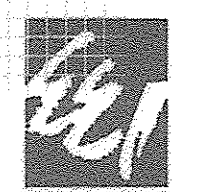
- ① NO PARKING, 1AM TO 6PM
- ② THREE HOUR PARKING, 8AM TO 6PM, MON - SAT
- ③ 30 MINUTE PARKING THIS SPACE ONLY
- ④ ST. CHARLES BORROMEO CATHOLIC SCHOOL

NOTE: ALL SIGNS TO BE MOUNTED TO THE LIGHT POLES WITH THE EXCEPTION OF THE ST. CHARLES BORROMEO CATHOLIC SCHOOL. SIGN MOUNTS TO BE PROVIDED BY THE POLE MANUFACTURER.



TYPICAL PARKING STALL DIMENSIONS
N.T.S.

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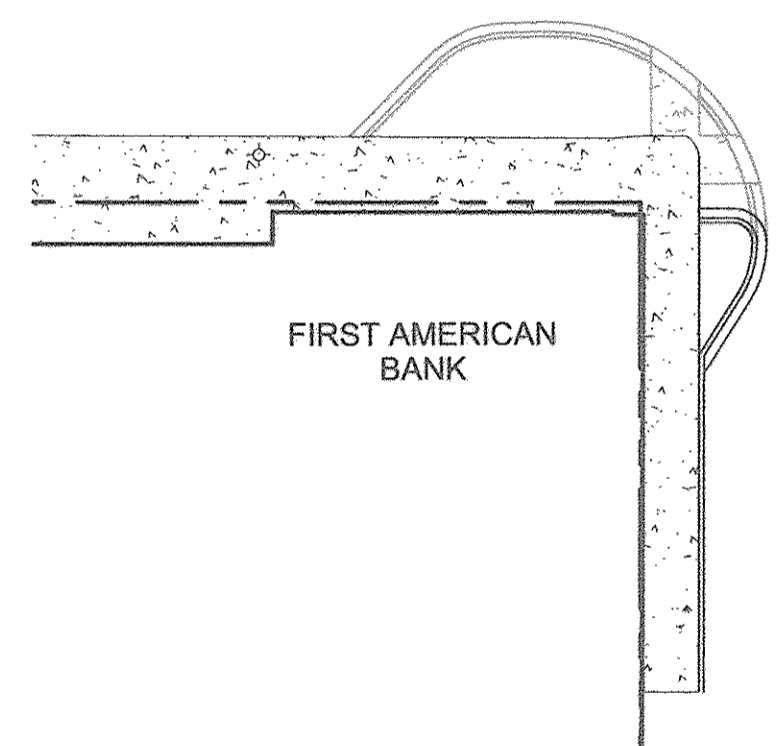
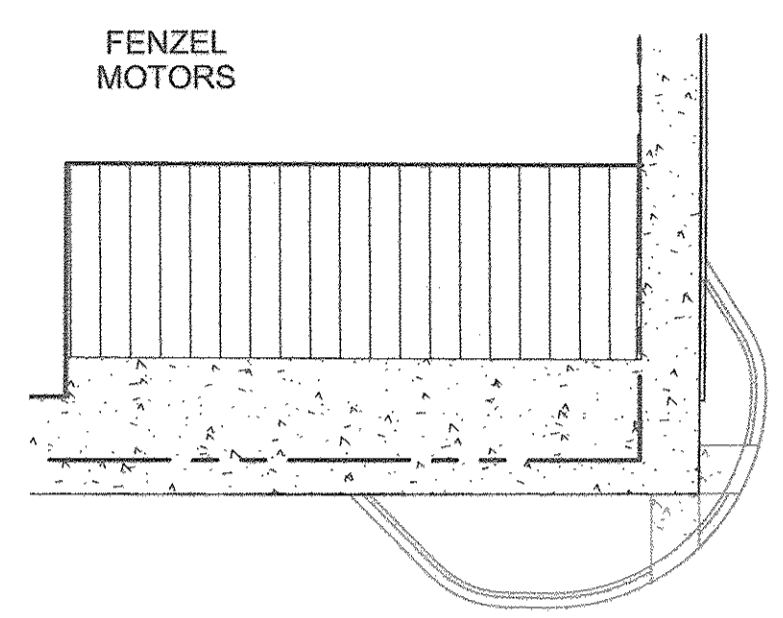
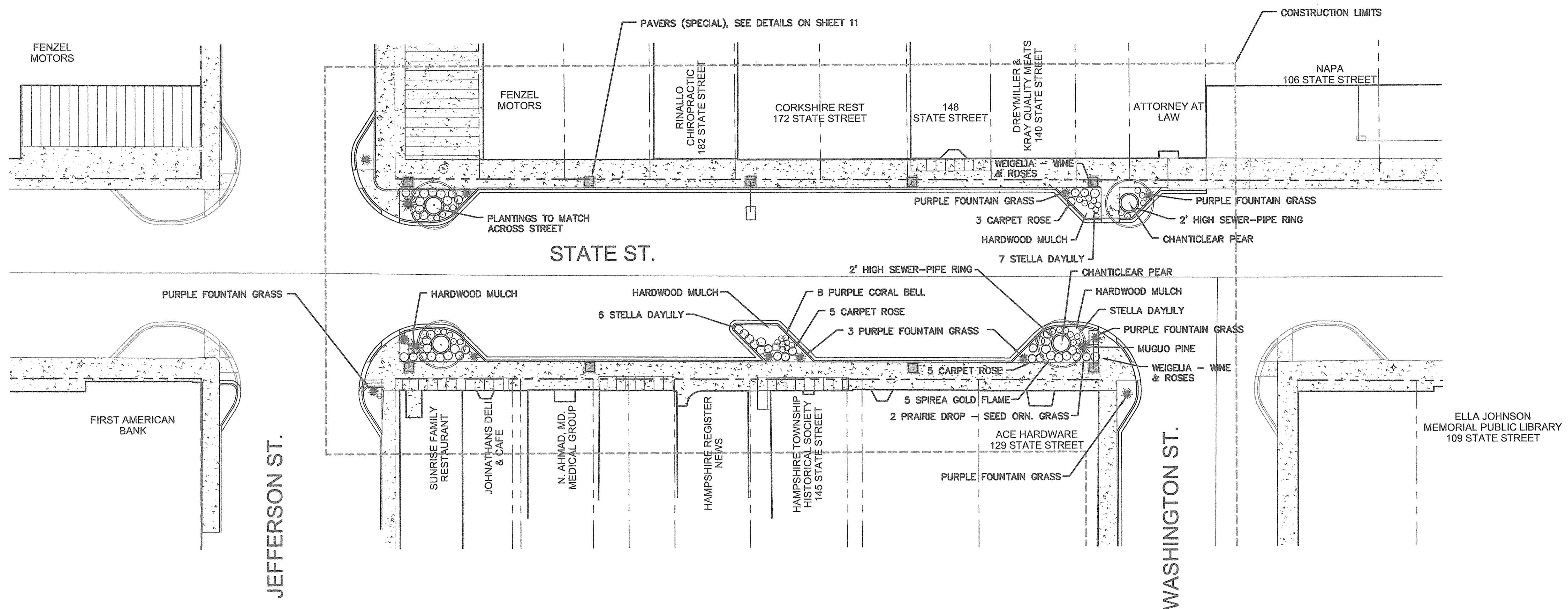
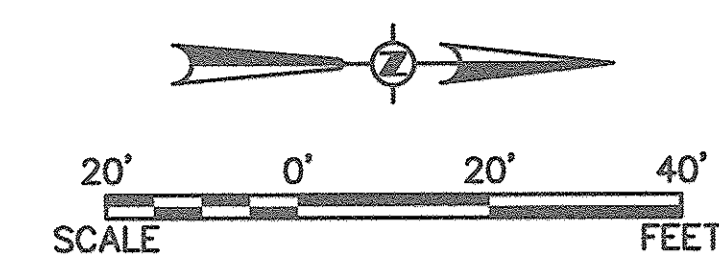
NO.	DATE	REVISIONS

**DOWNTOWN IMPROVEMENTS
PHASE 1**

PAVEMENT MARKING PLAN

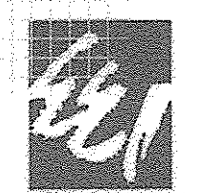
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DATE	JUNE 2003
PROJ. NO.	HA0247
FILE NO.	HA024712
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- NOTE:**
1. PAVER COLOR TO BE DETERMINED PRIOR TO INSTALLATION
 2. PAVER PATTERN TO BE DETERMINED PRIOR TO INSTALLATION

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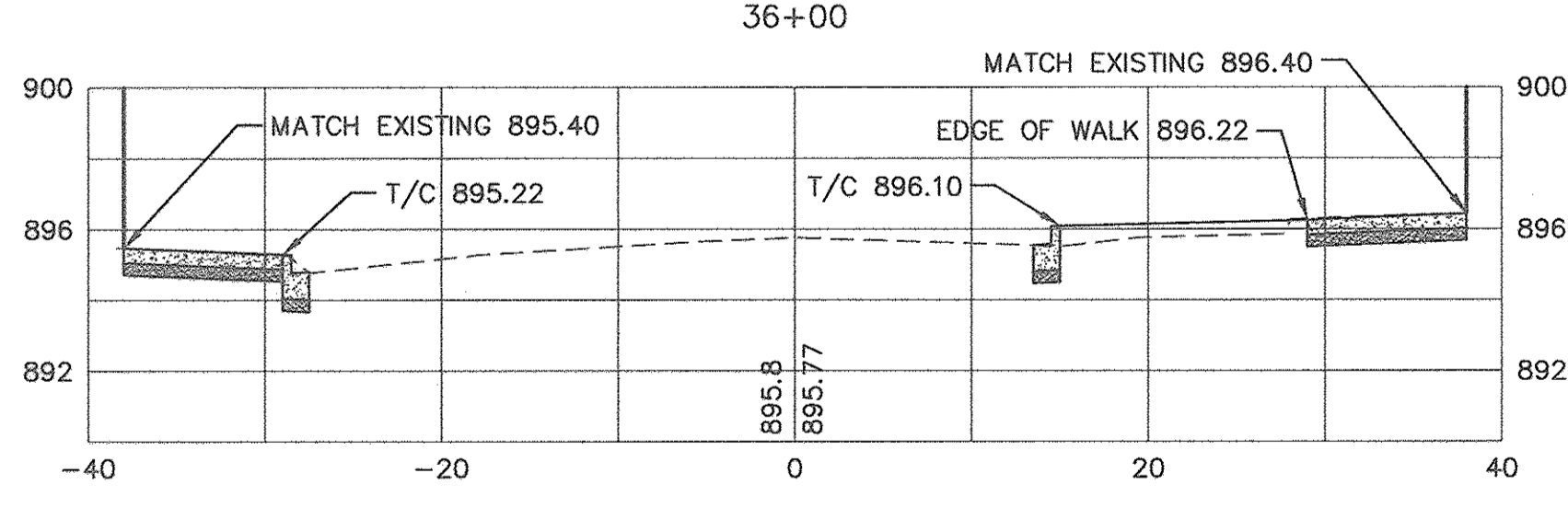
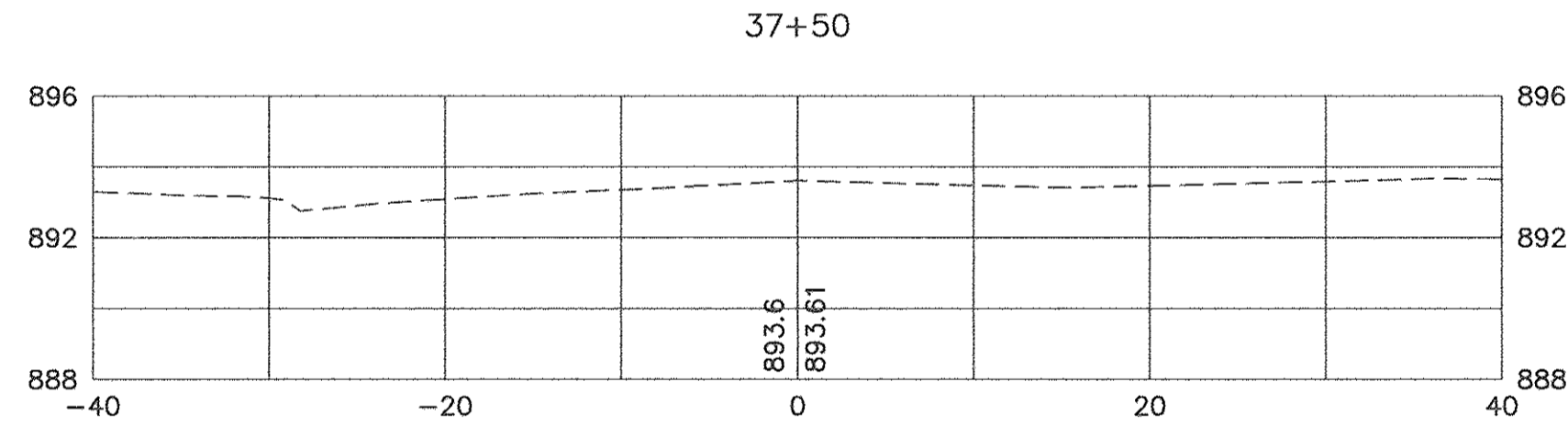
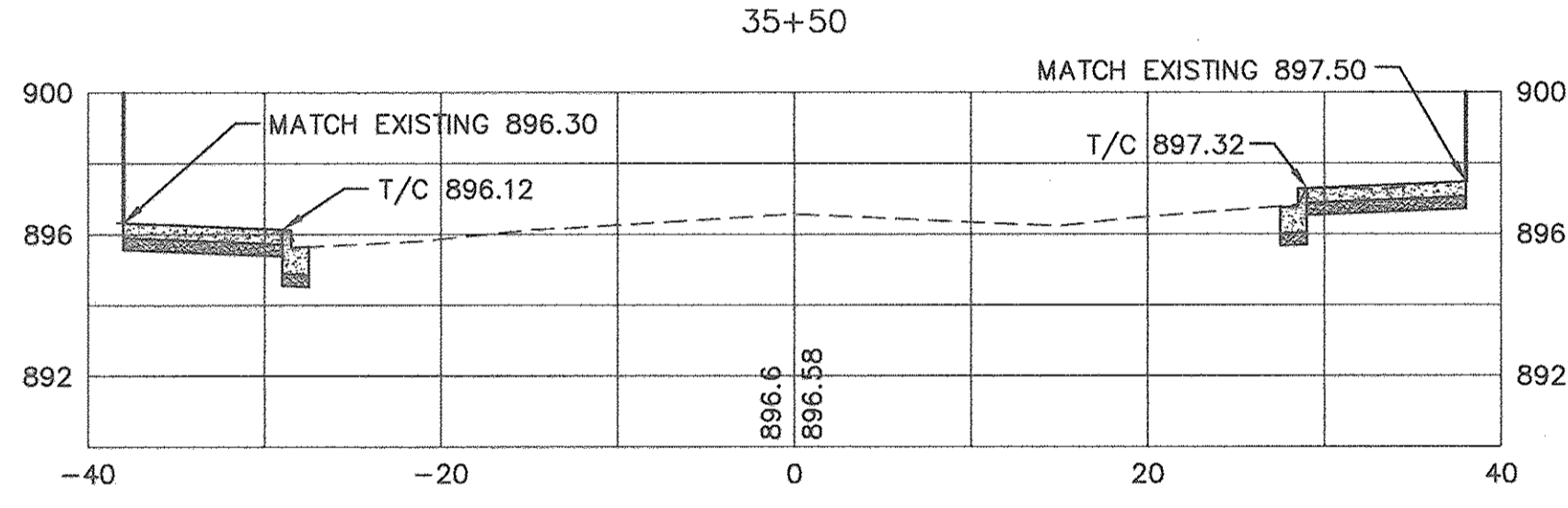
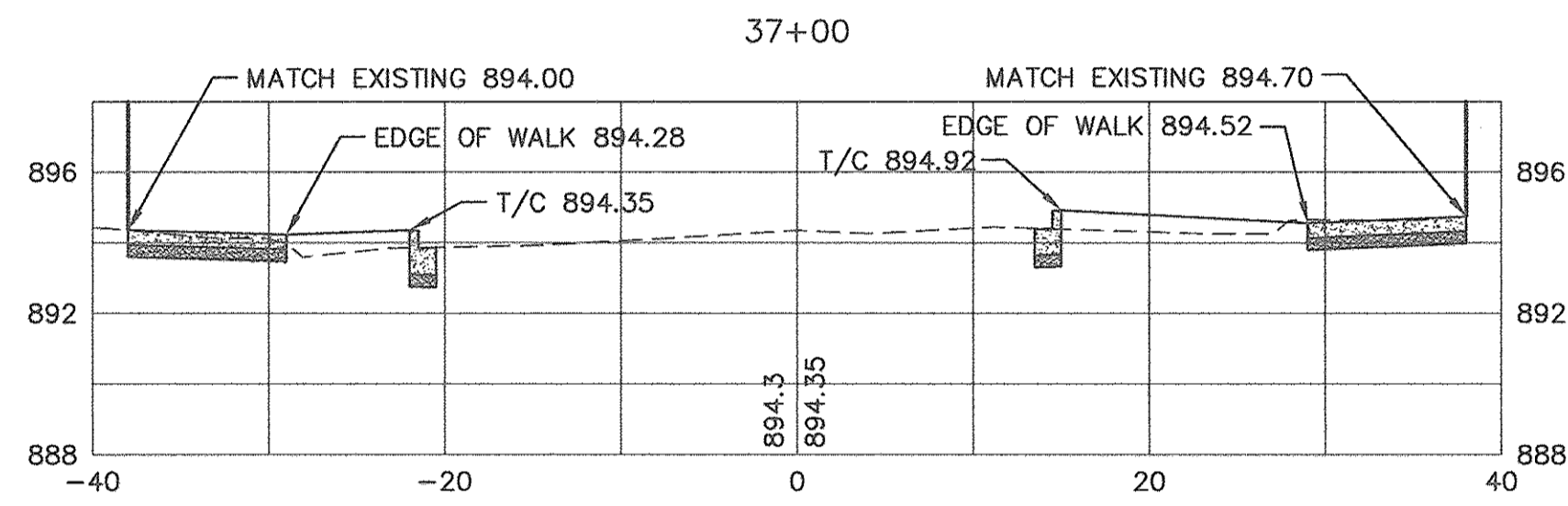
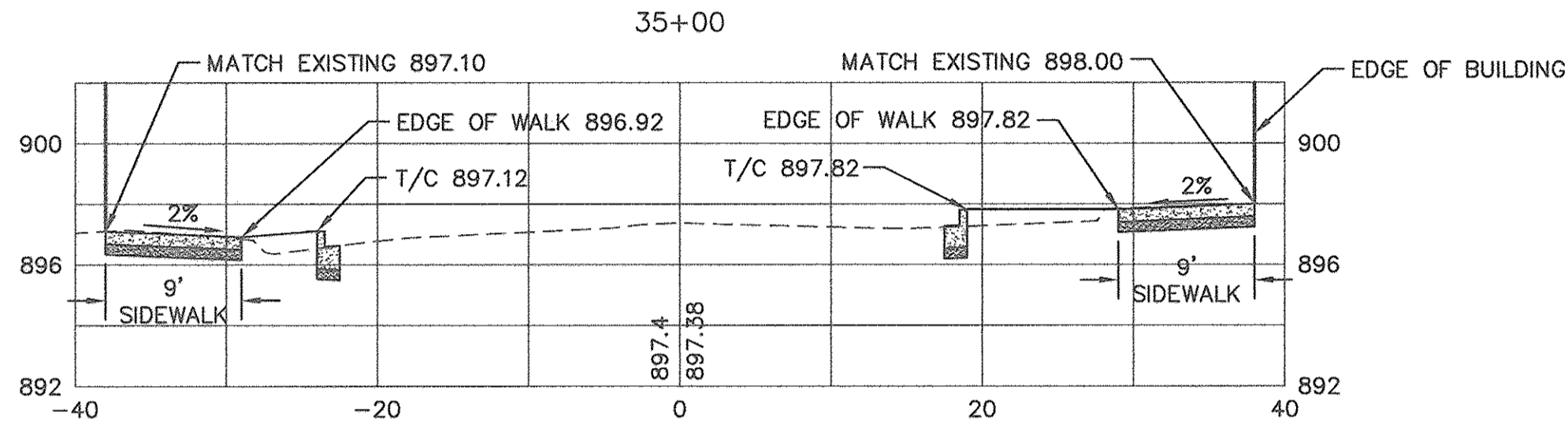
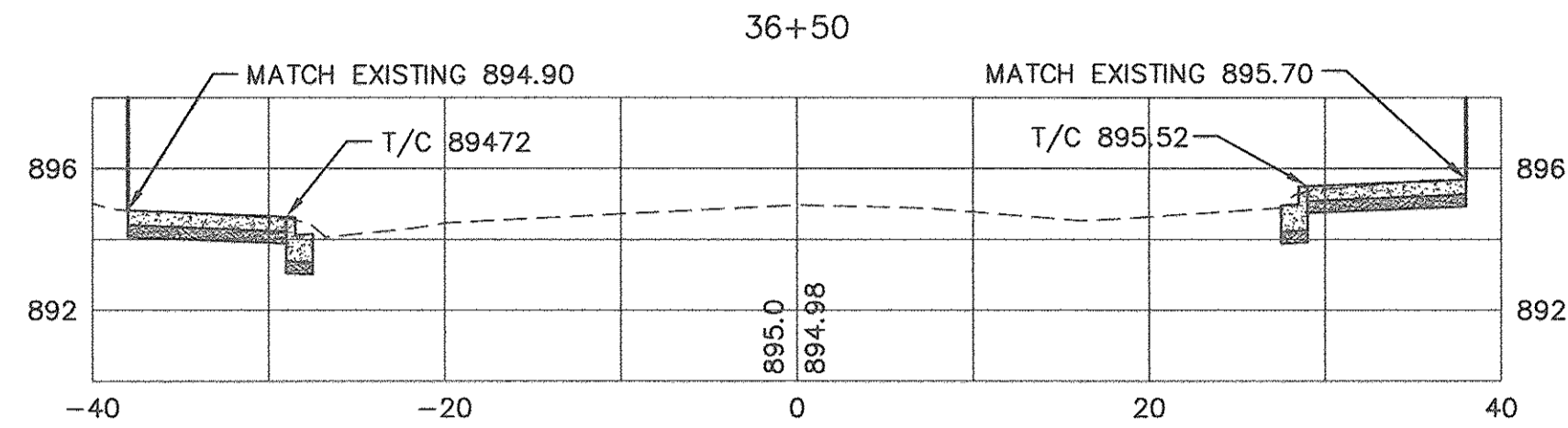
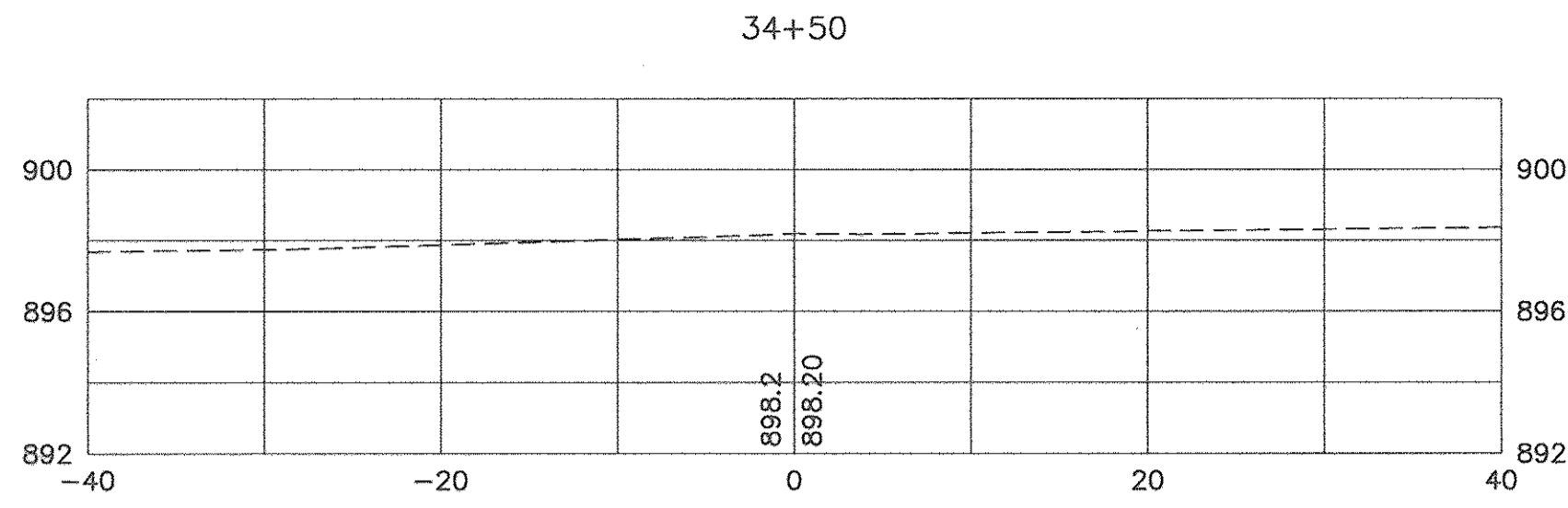
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**DOWNTOWN IMPROVEMENTS
 PHASE 1**

LANDSCAPE PLAN

DATE	JUNE 2003
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FILE NO.	HA024711
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SCALE:
 HORZ. 1" = 10'
 VERT. 1" = 5'



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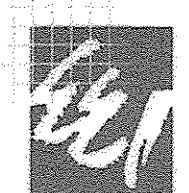
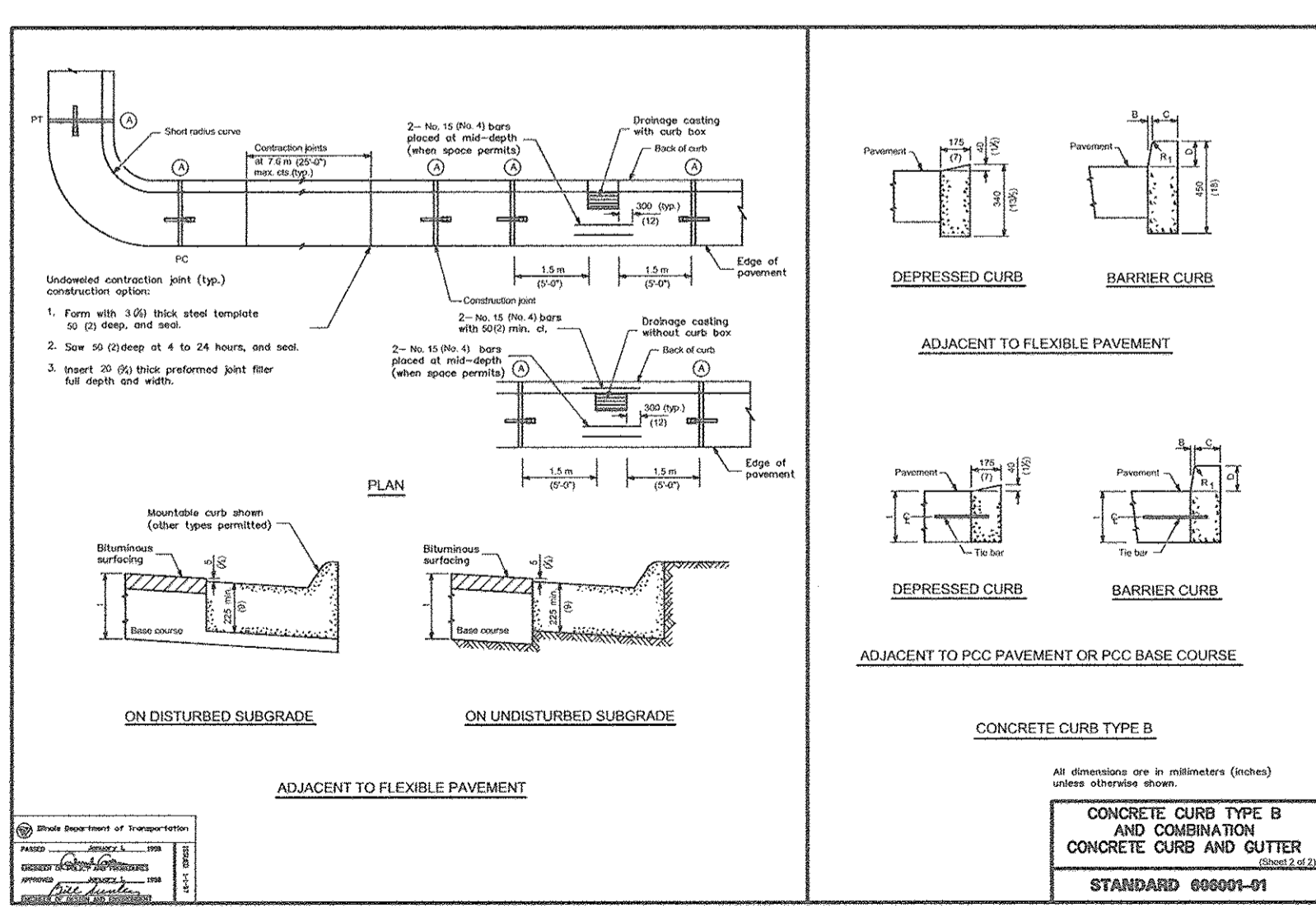
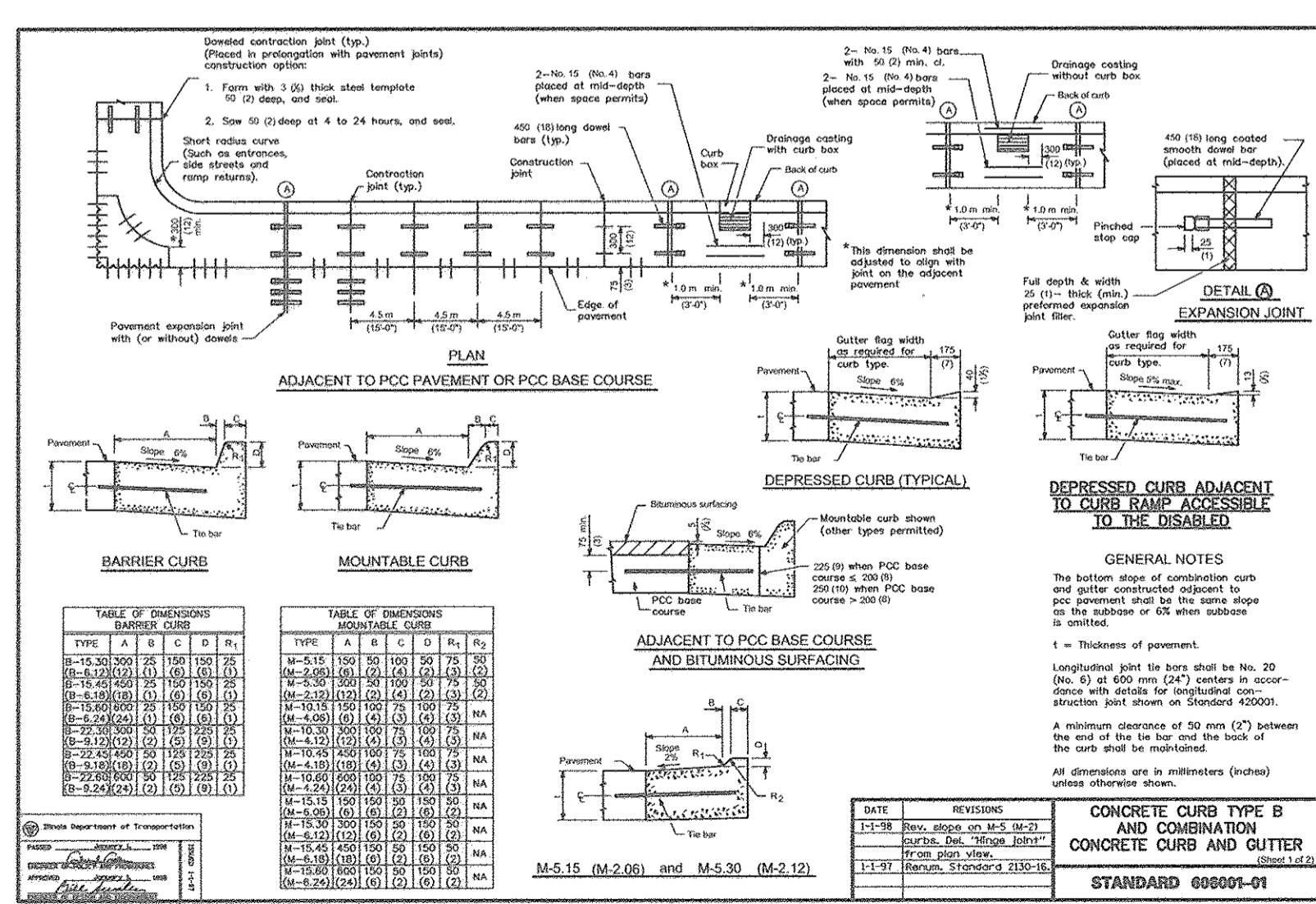
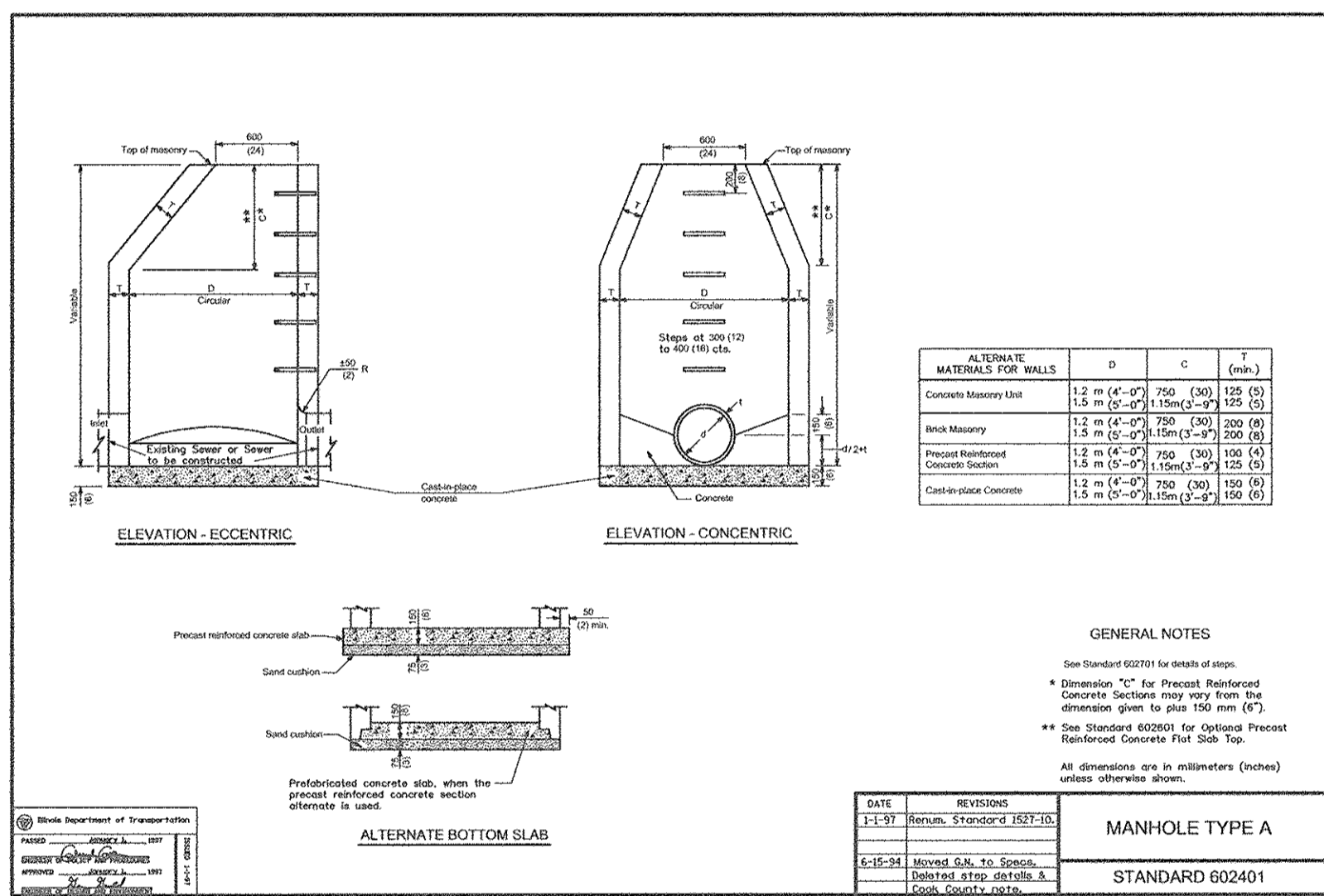
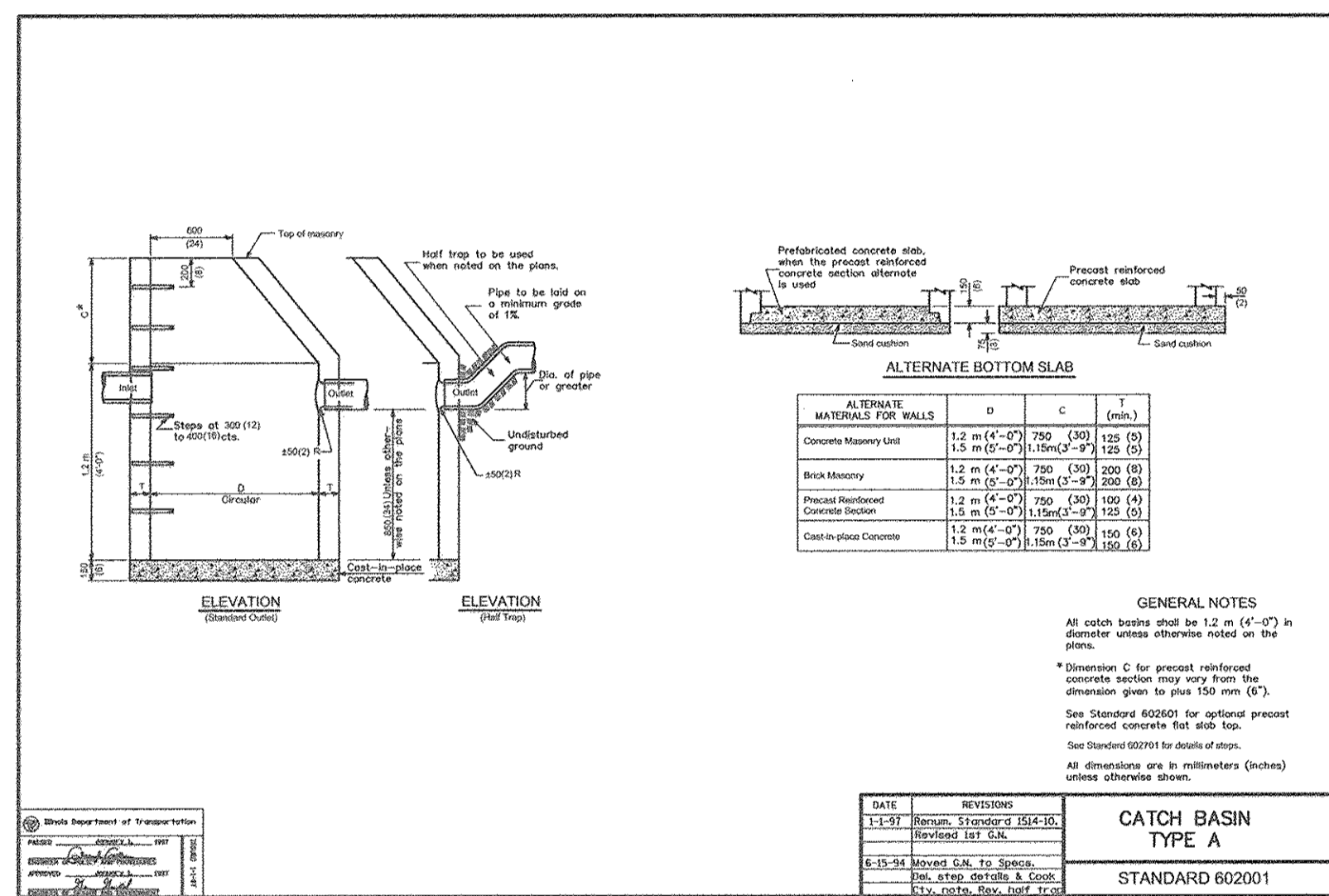
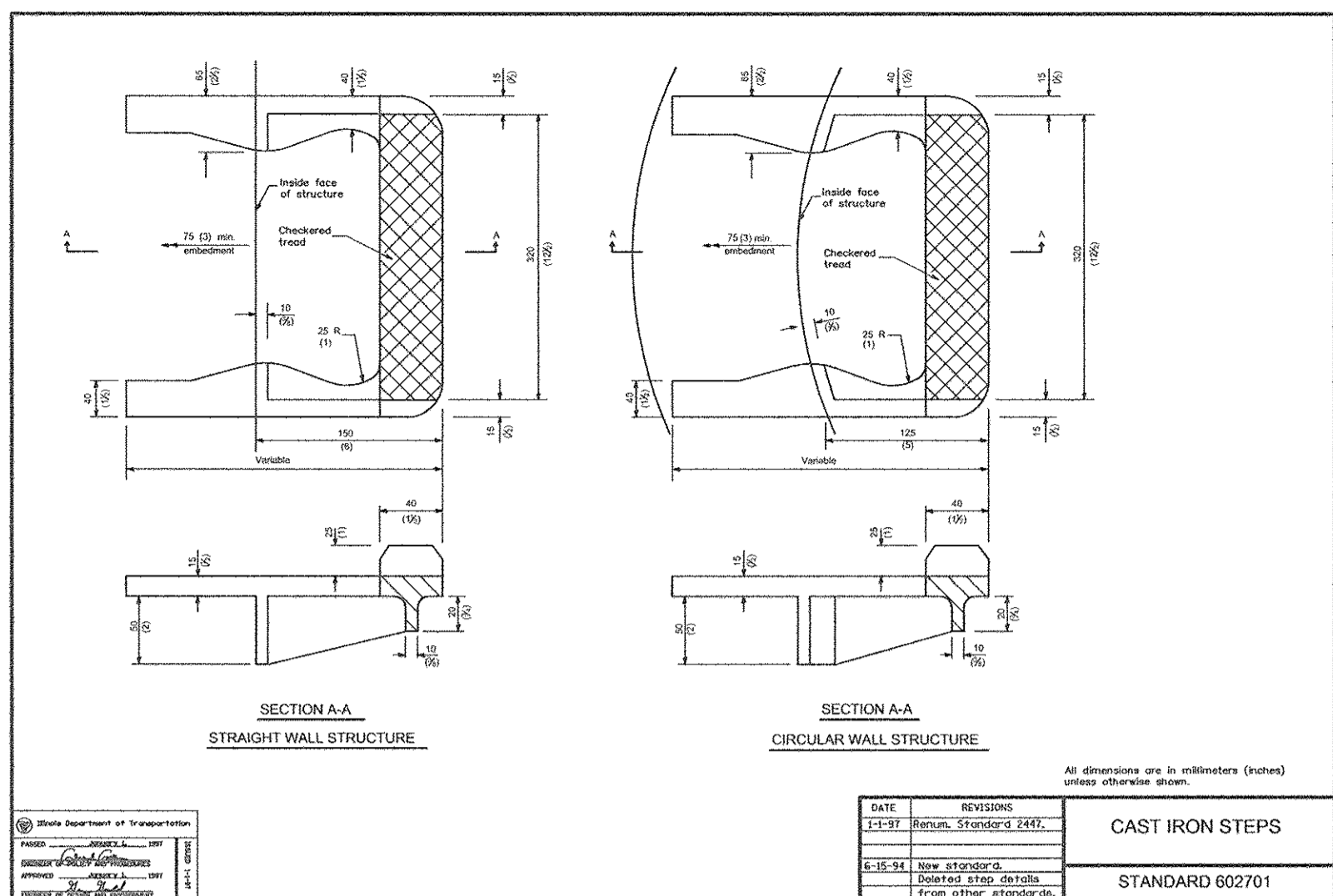
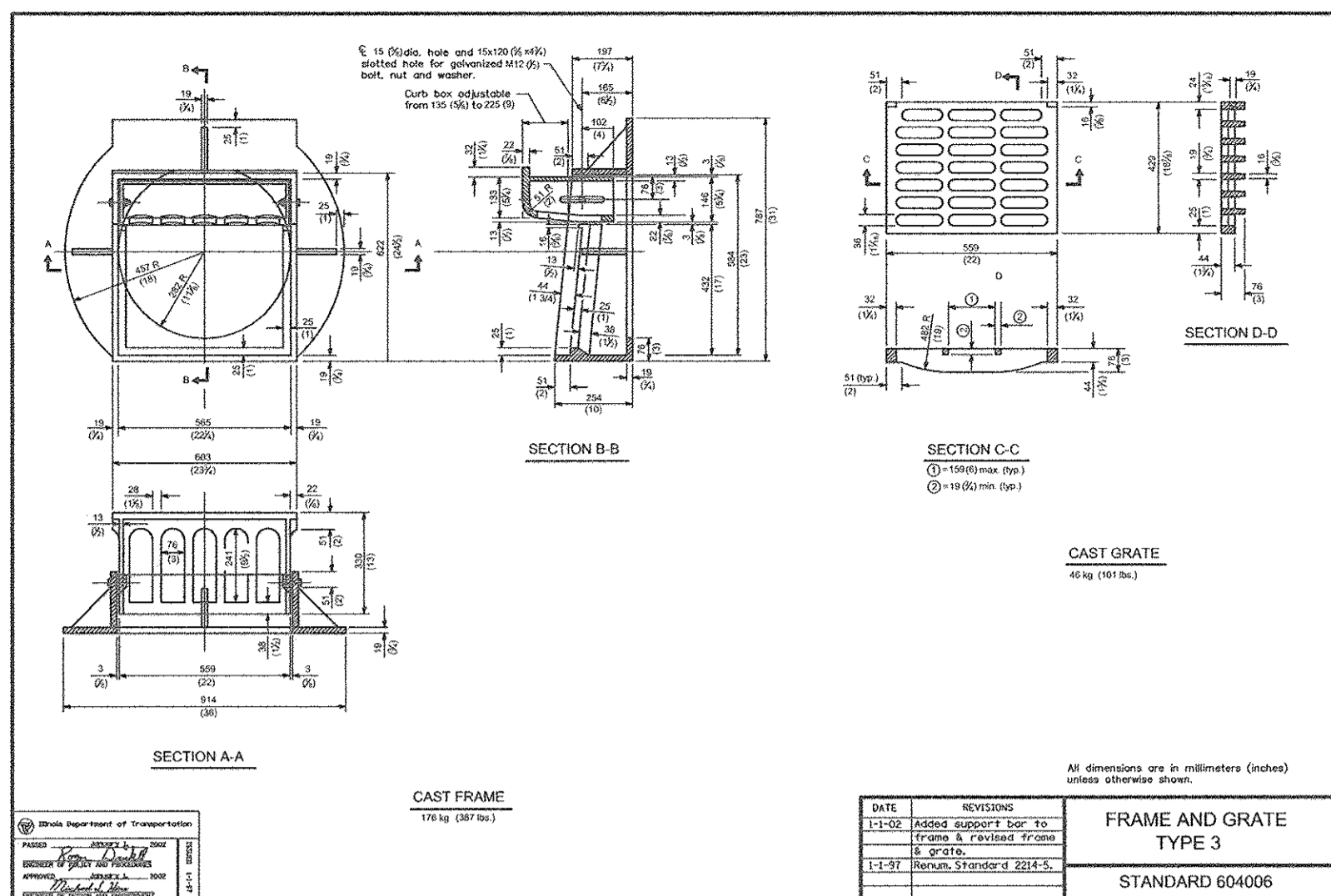
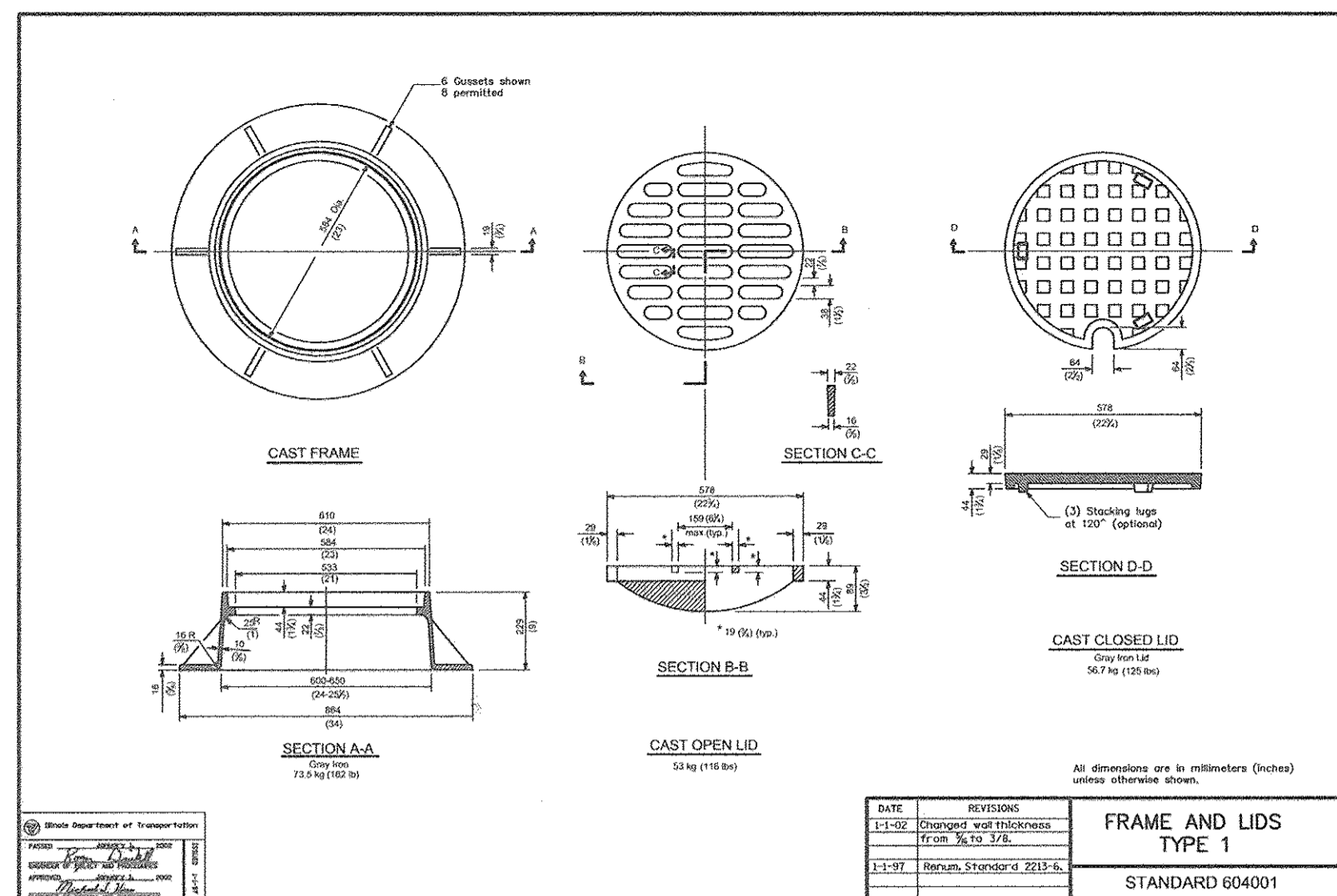
VILLAGE OF HAMPSHIRE
 KANE COUNTY, ILLINOIS

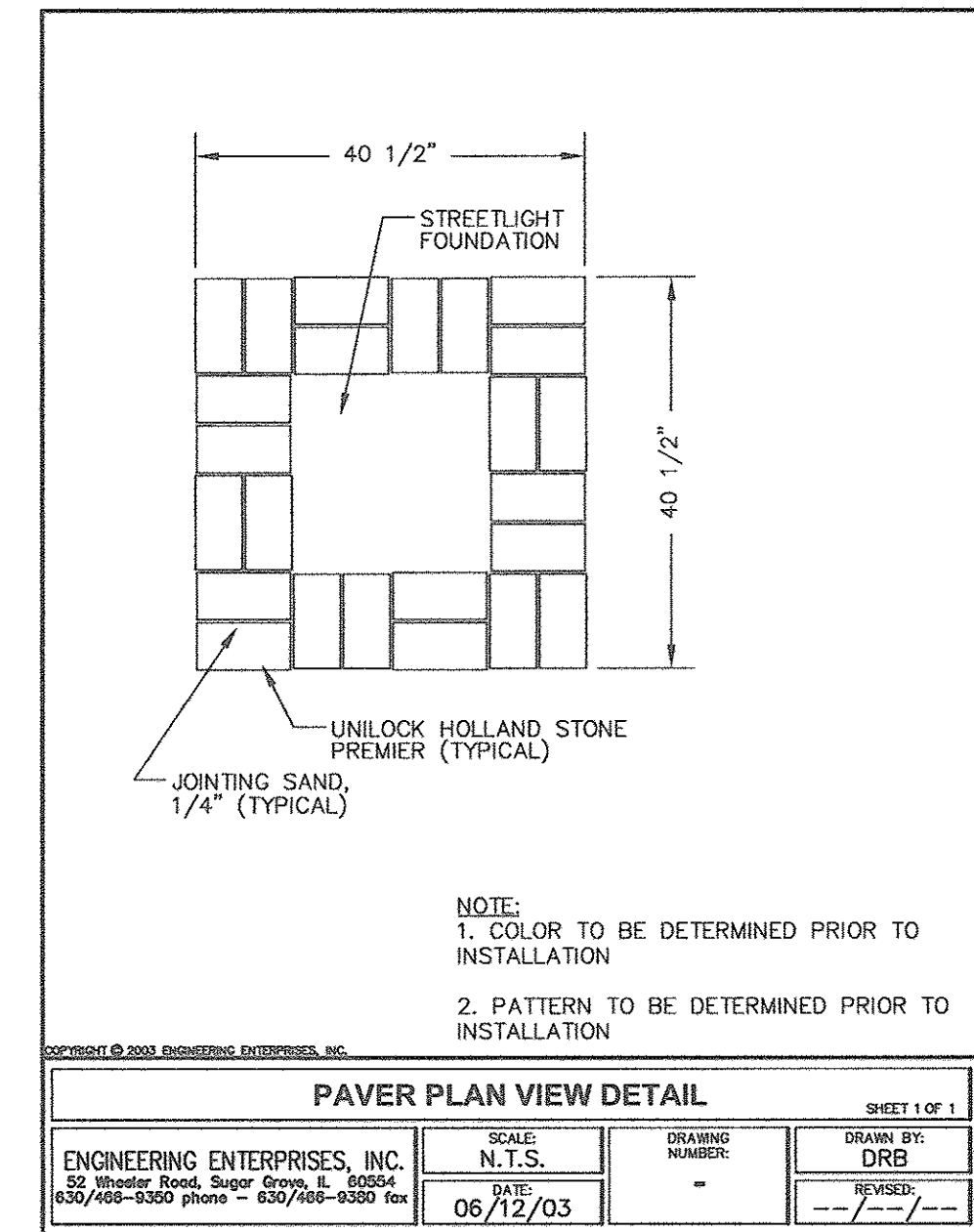
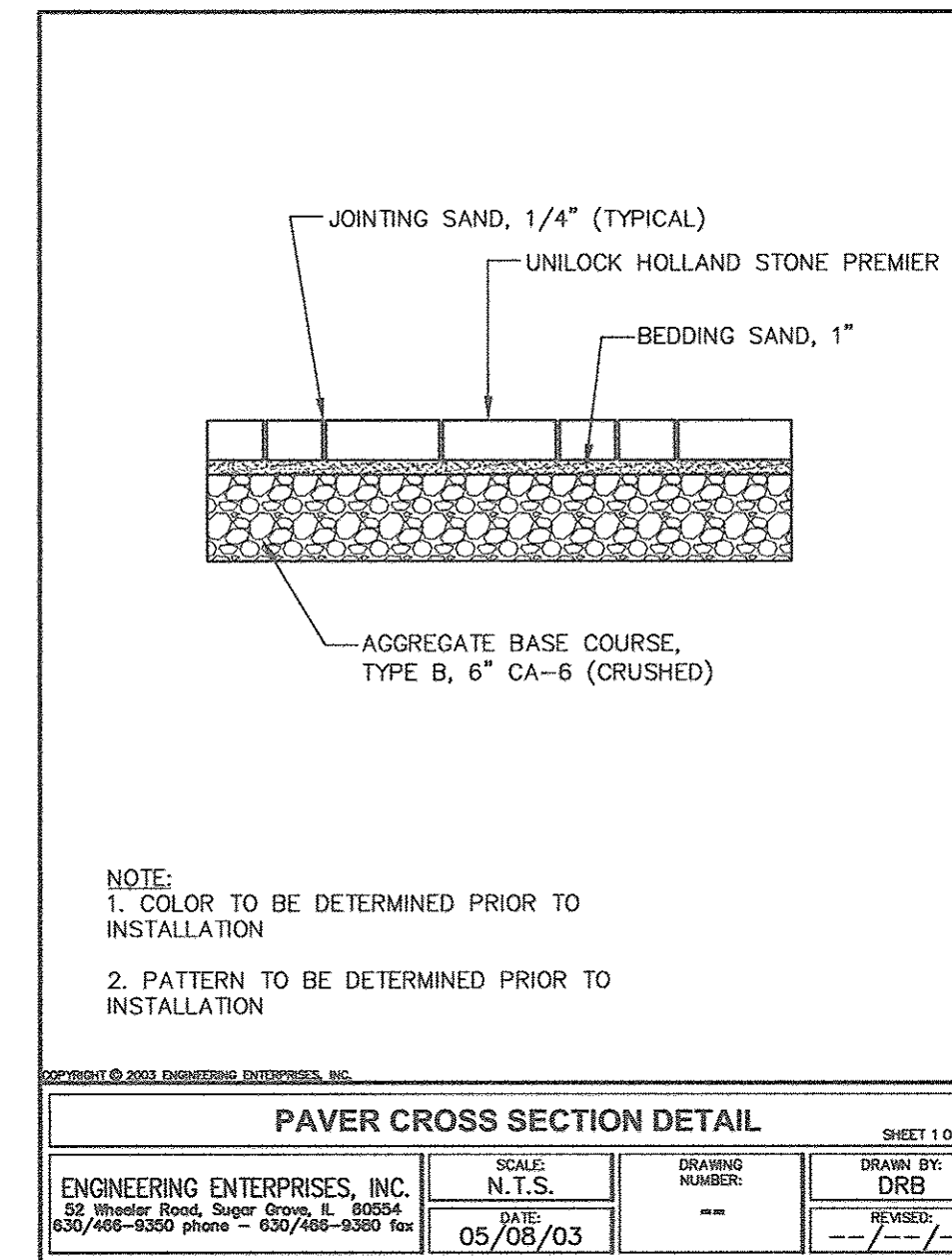
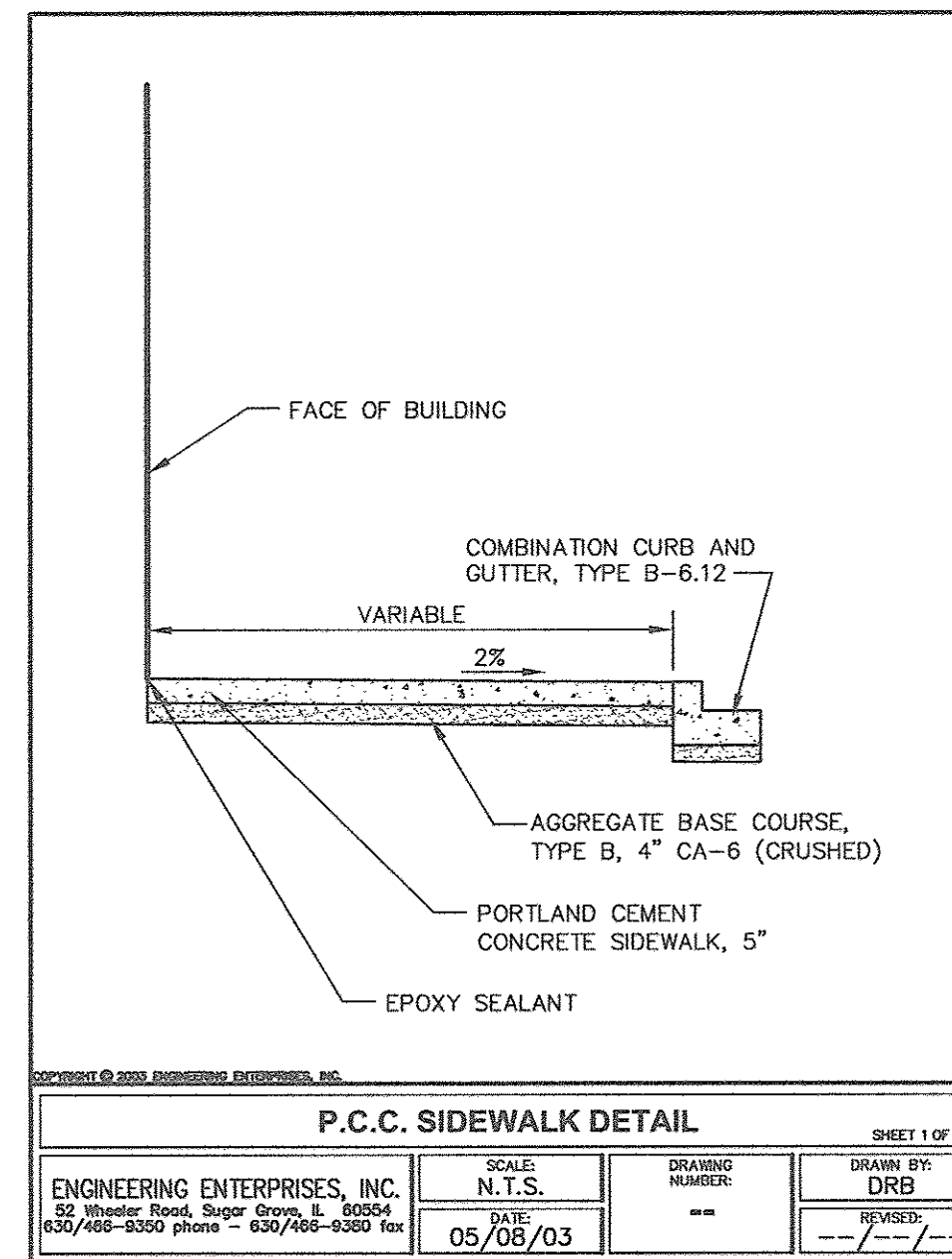
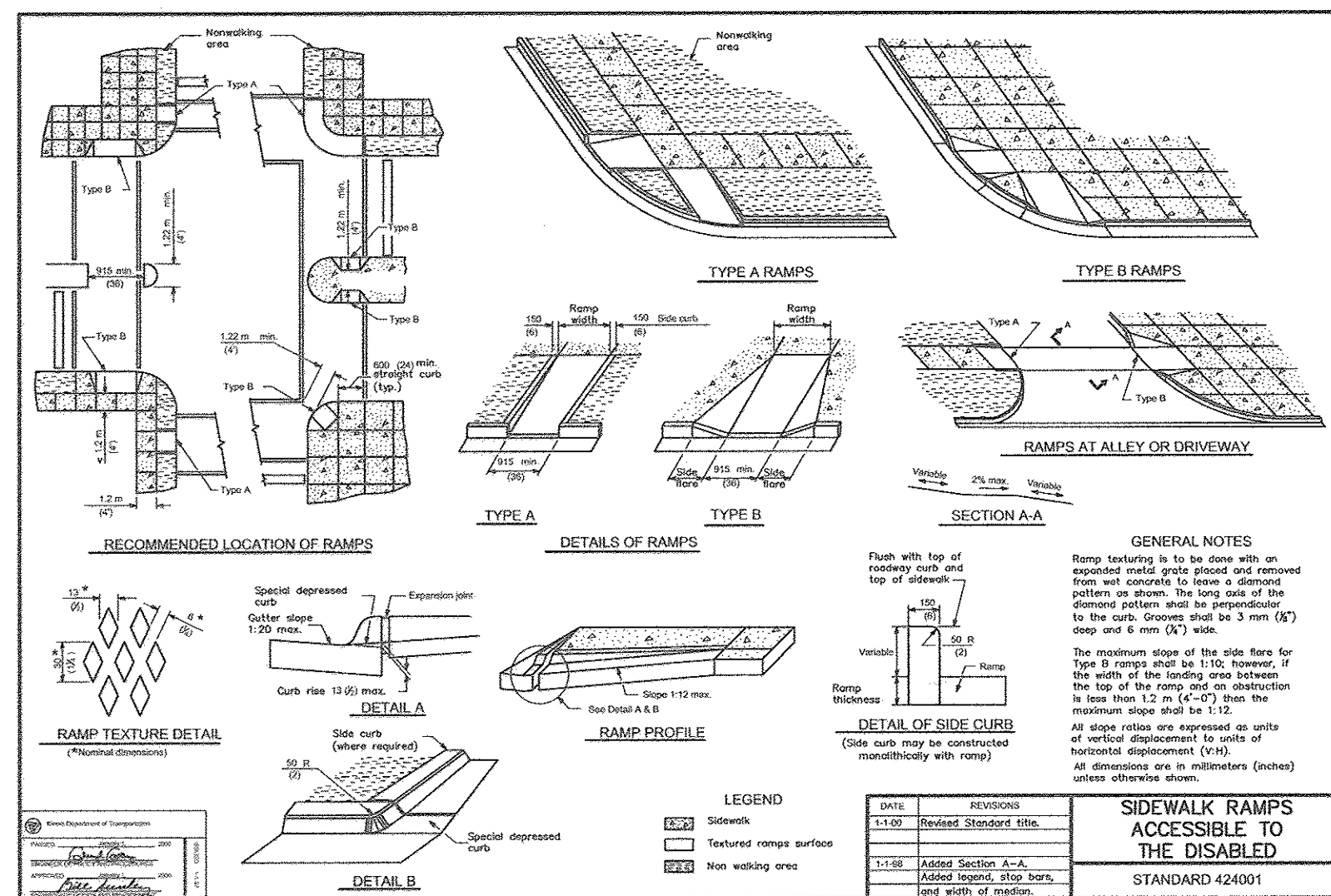
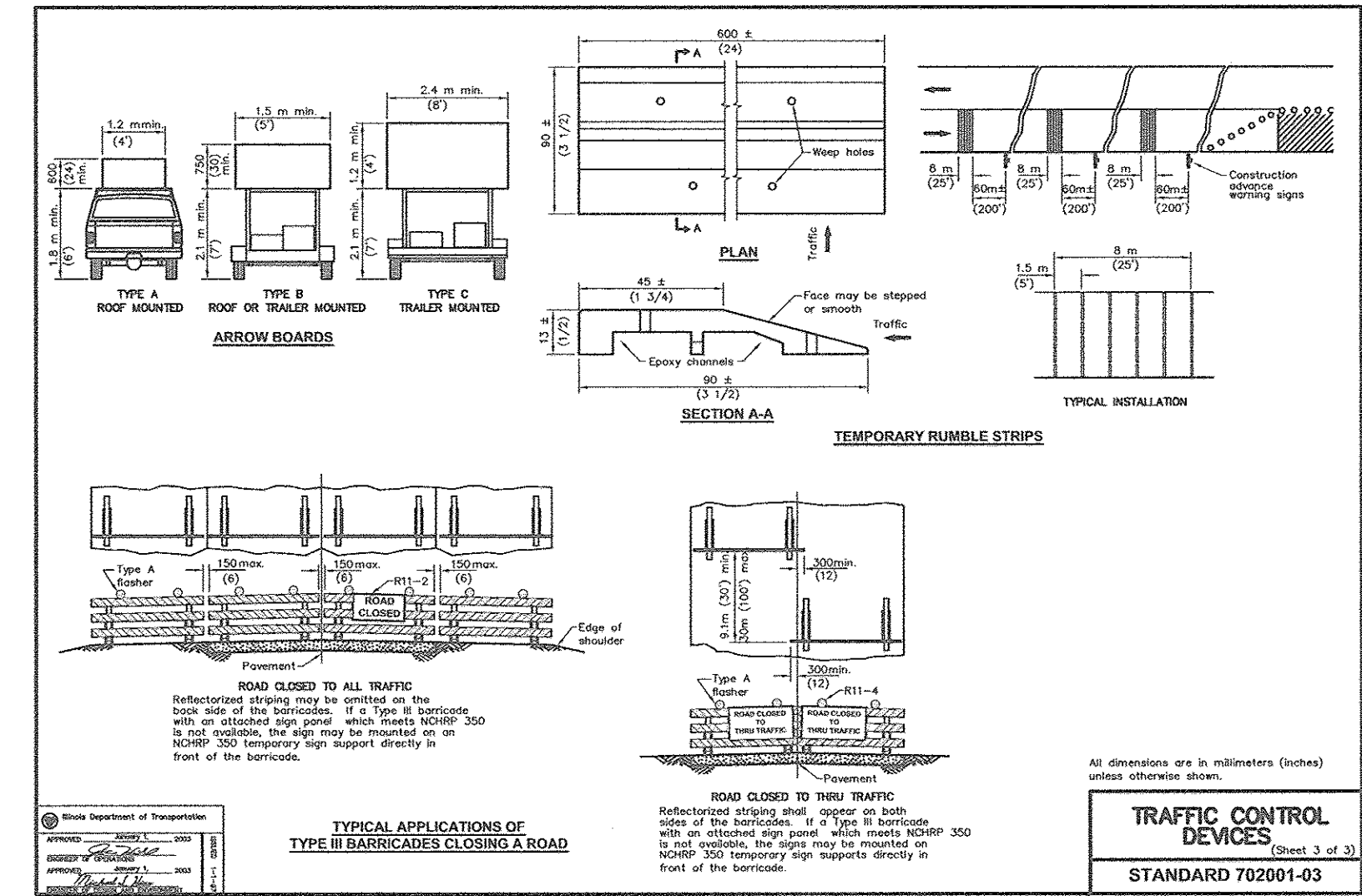
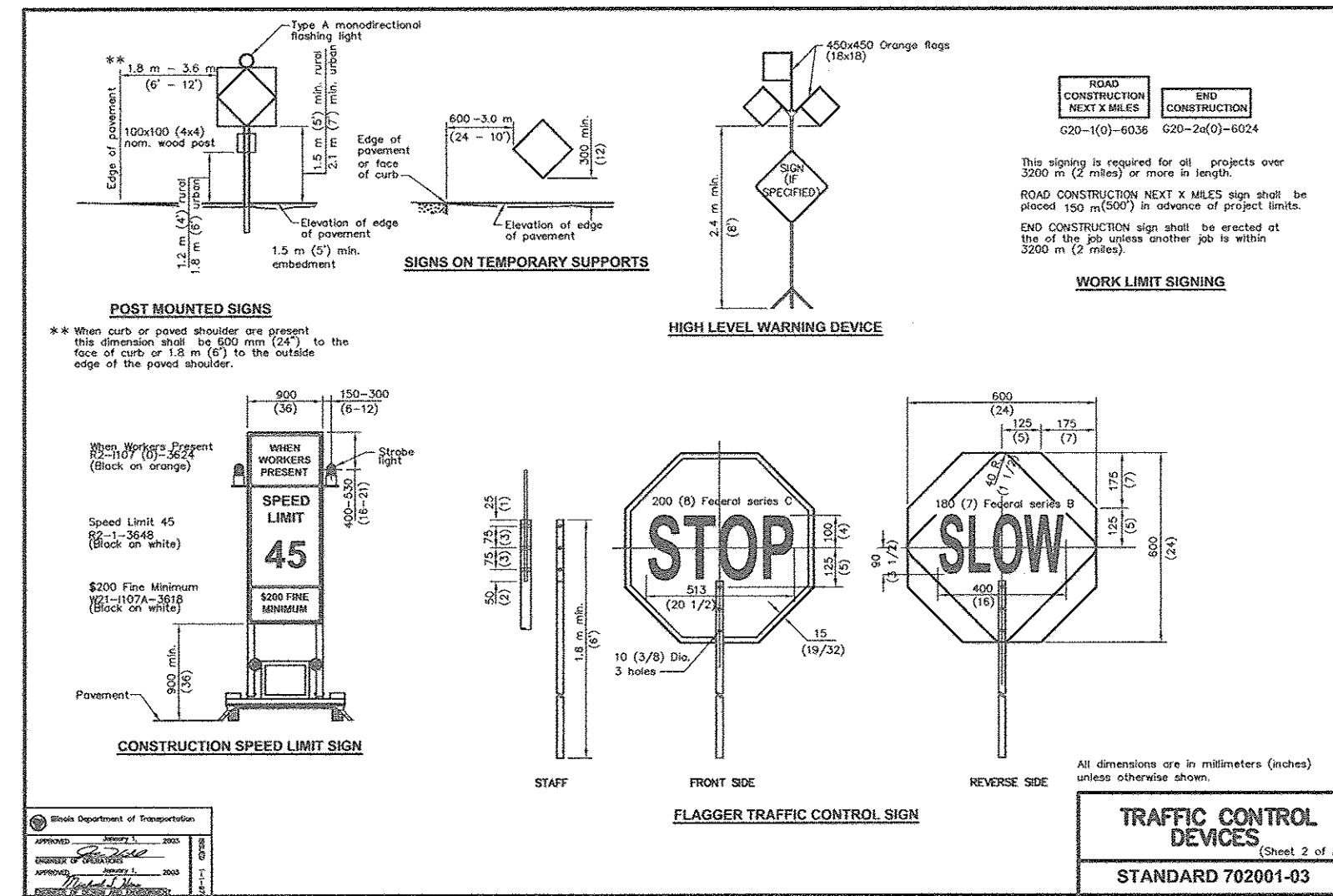
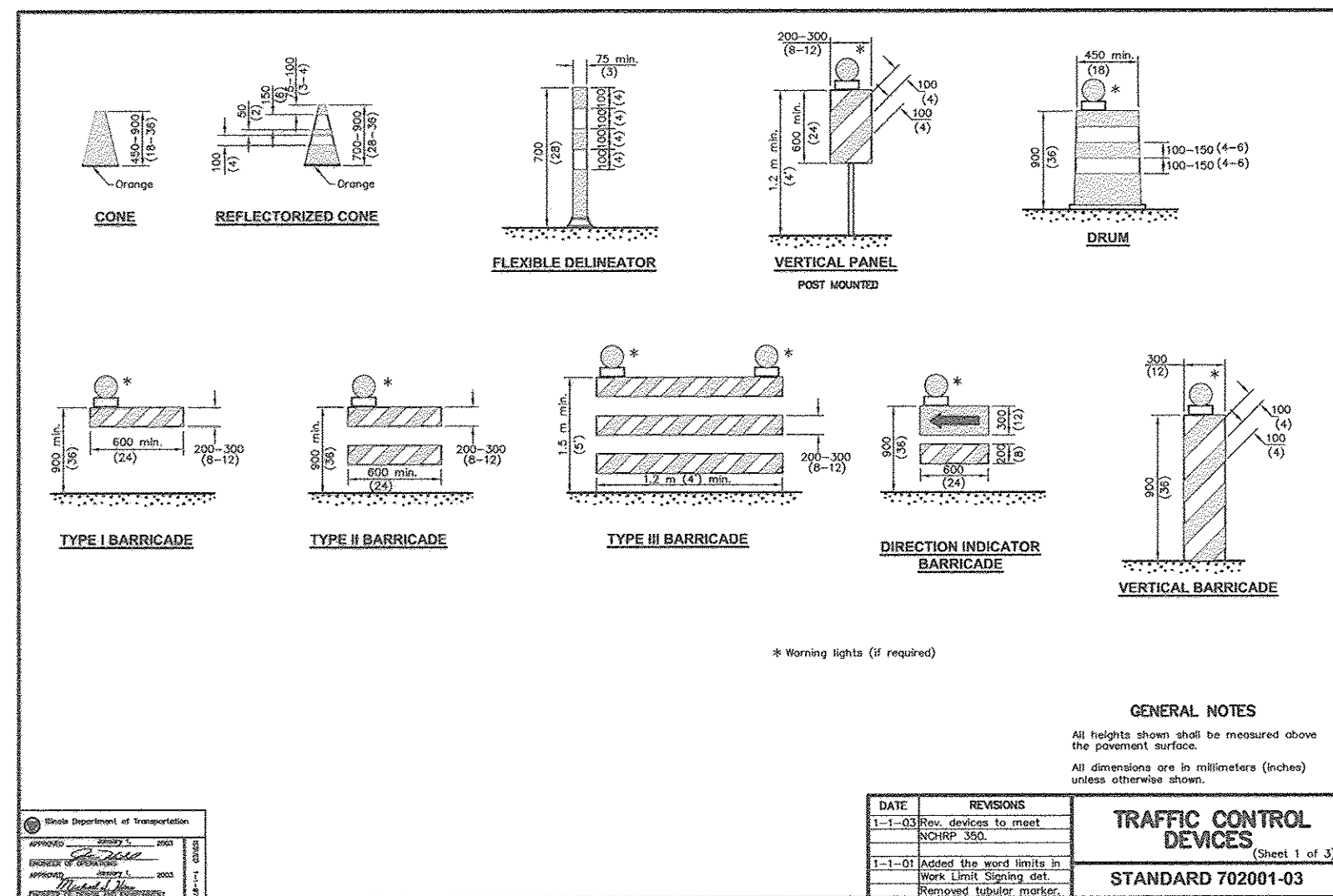
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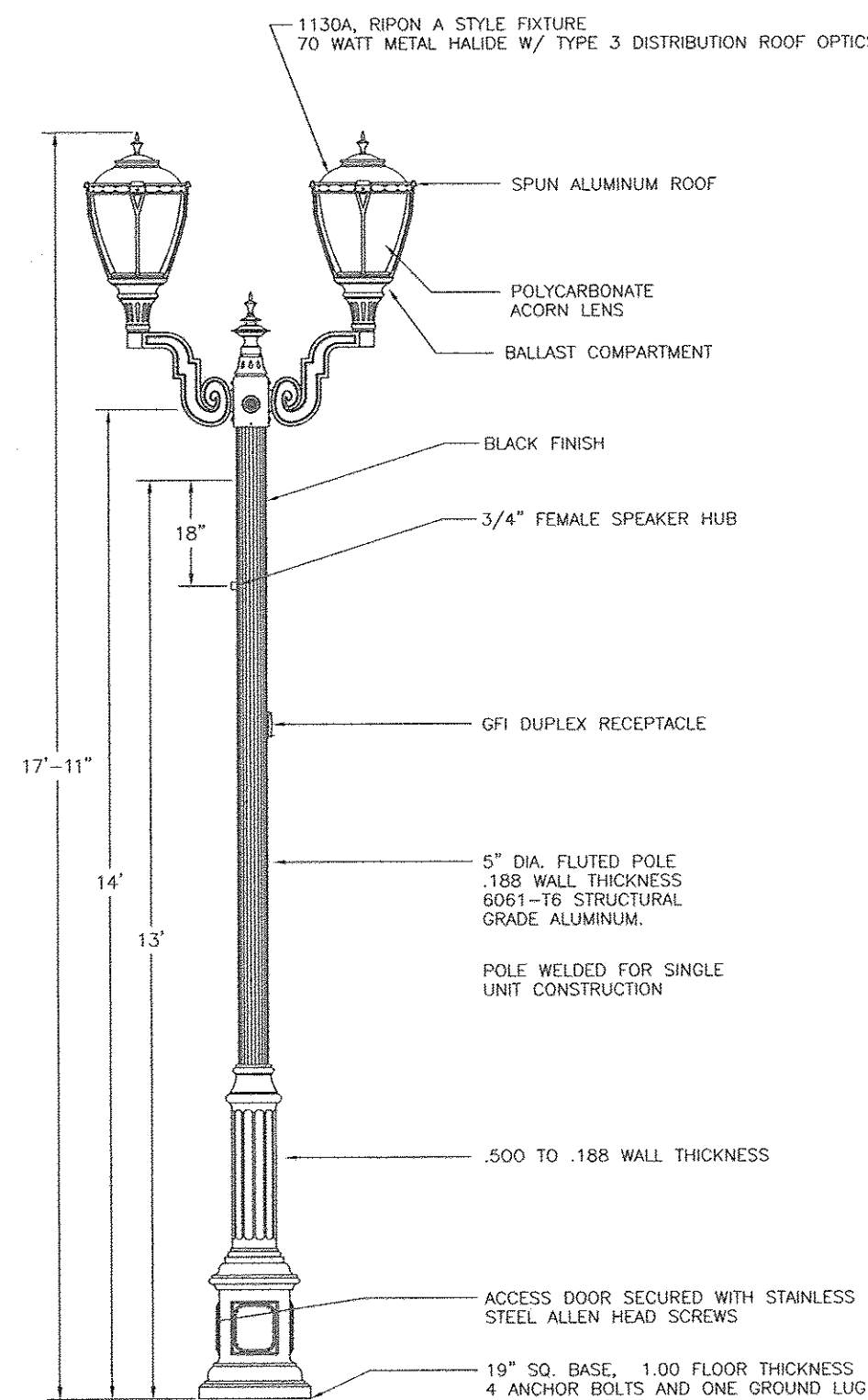
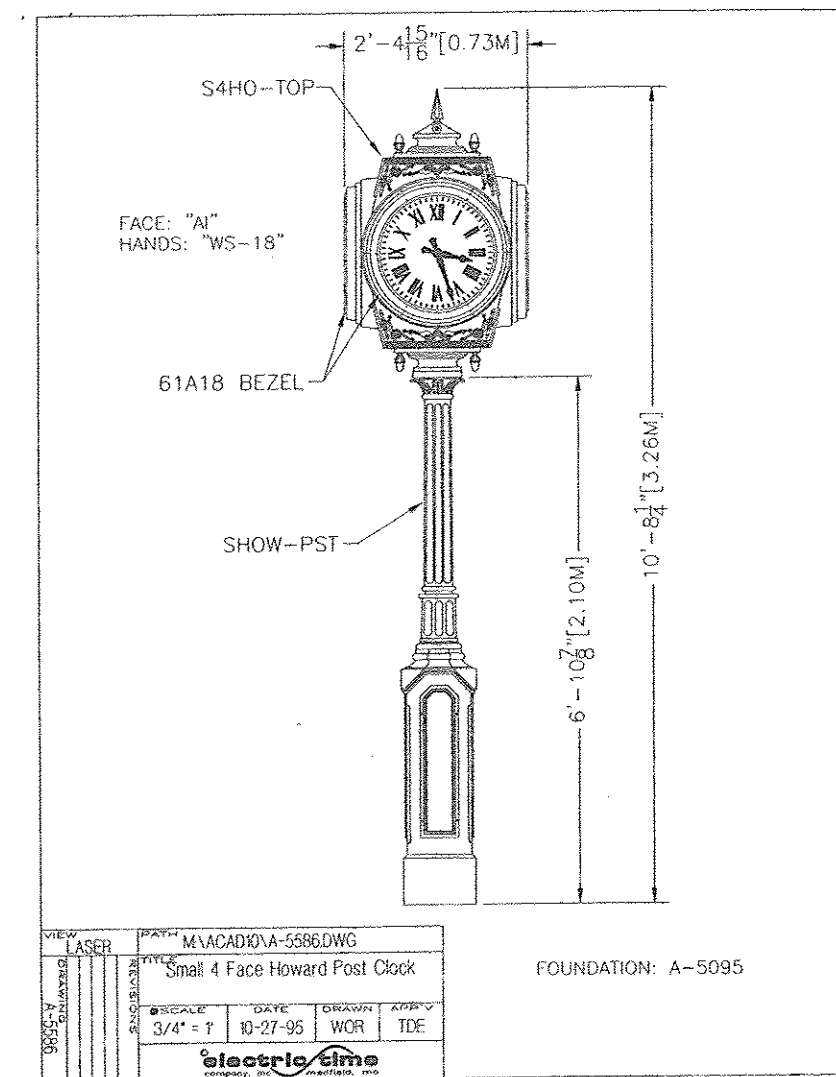
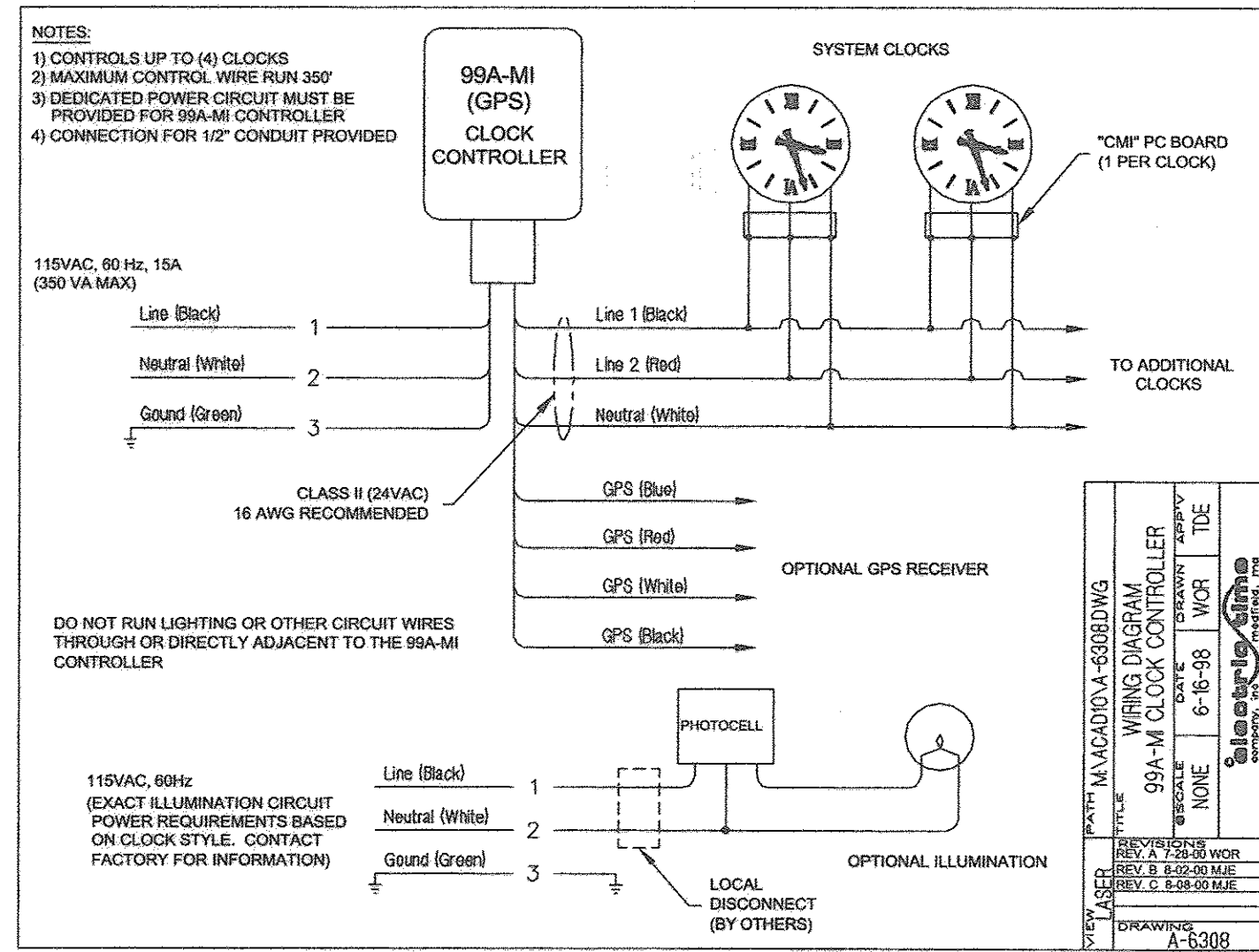
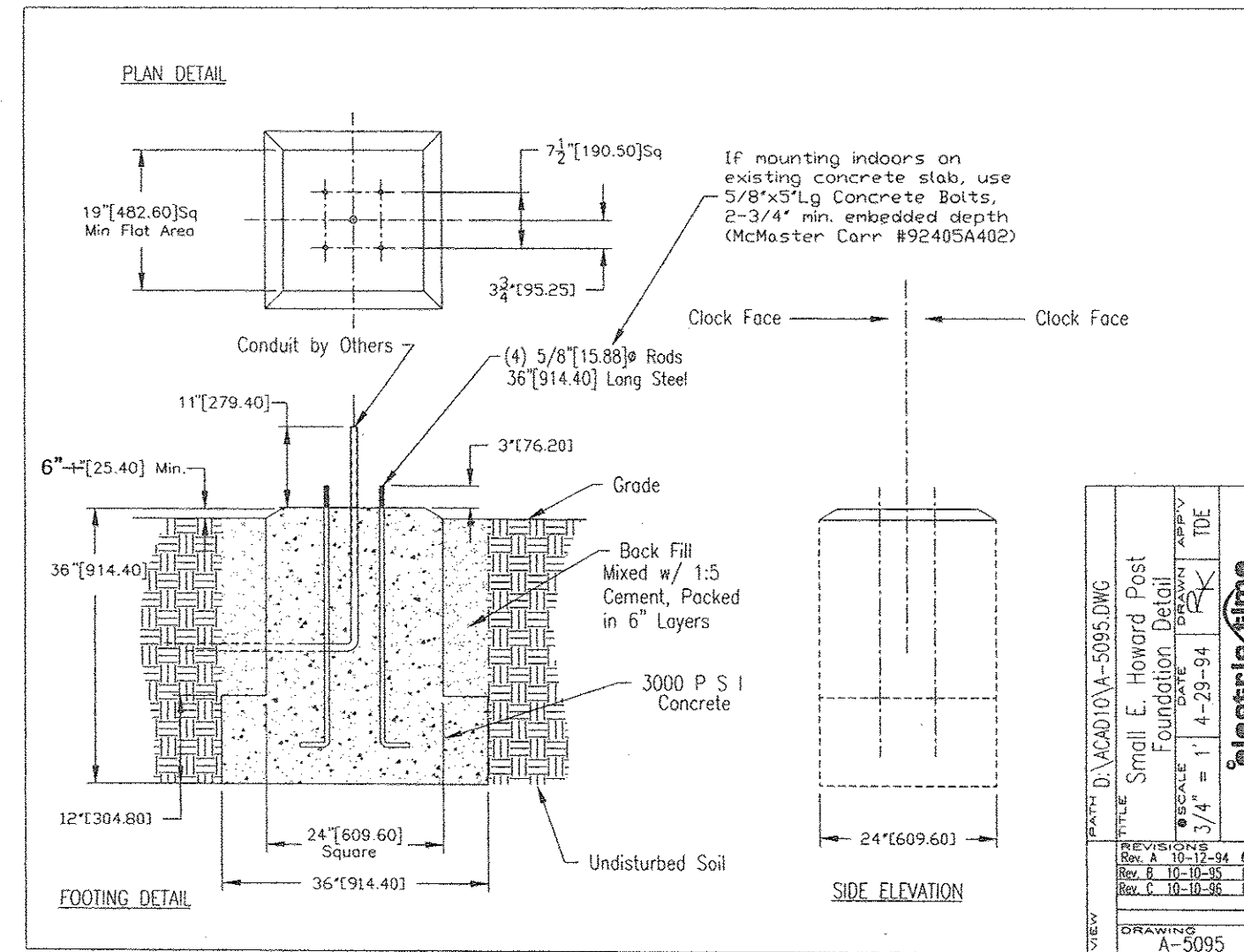
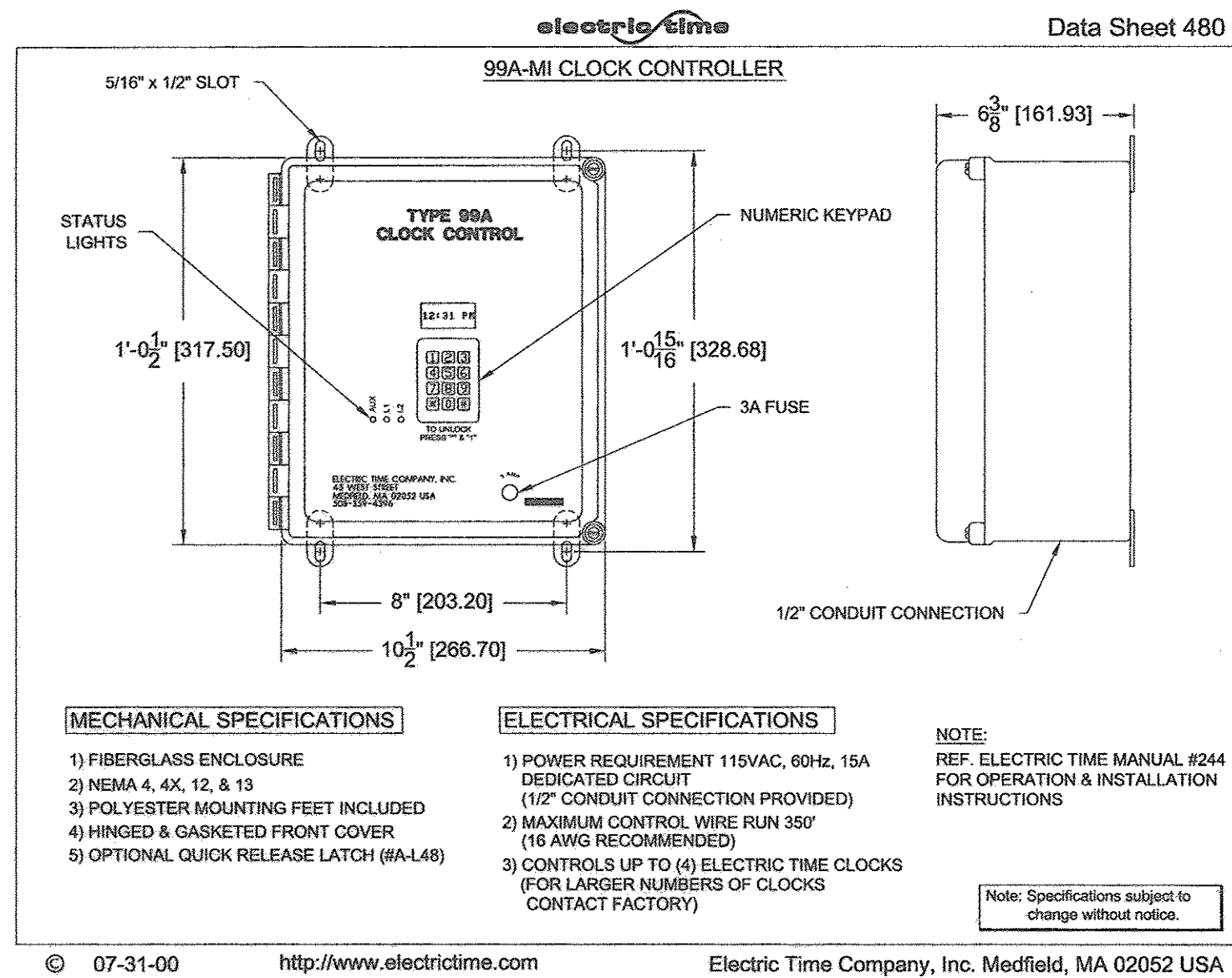
DOWNTOWN IMPROVEMENTS
 PHASE 1

CROSS SECTIONS

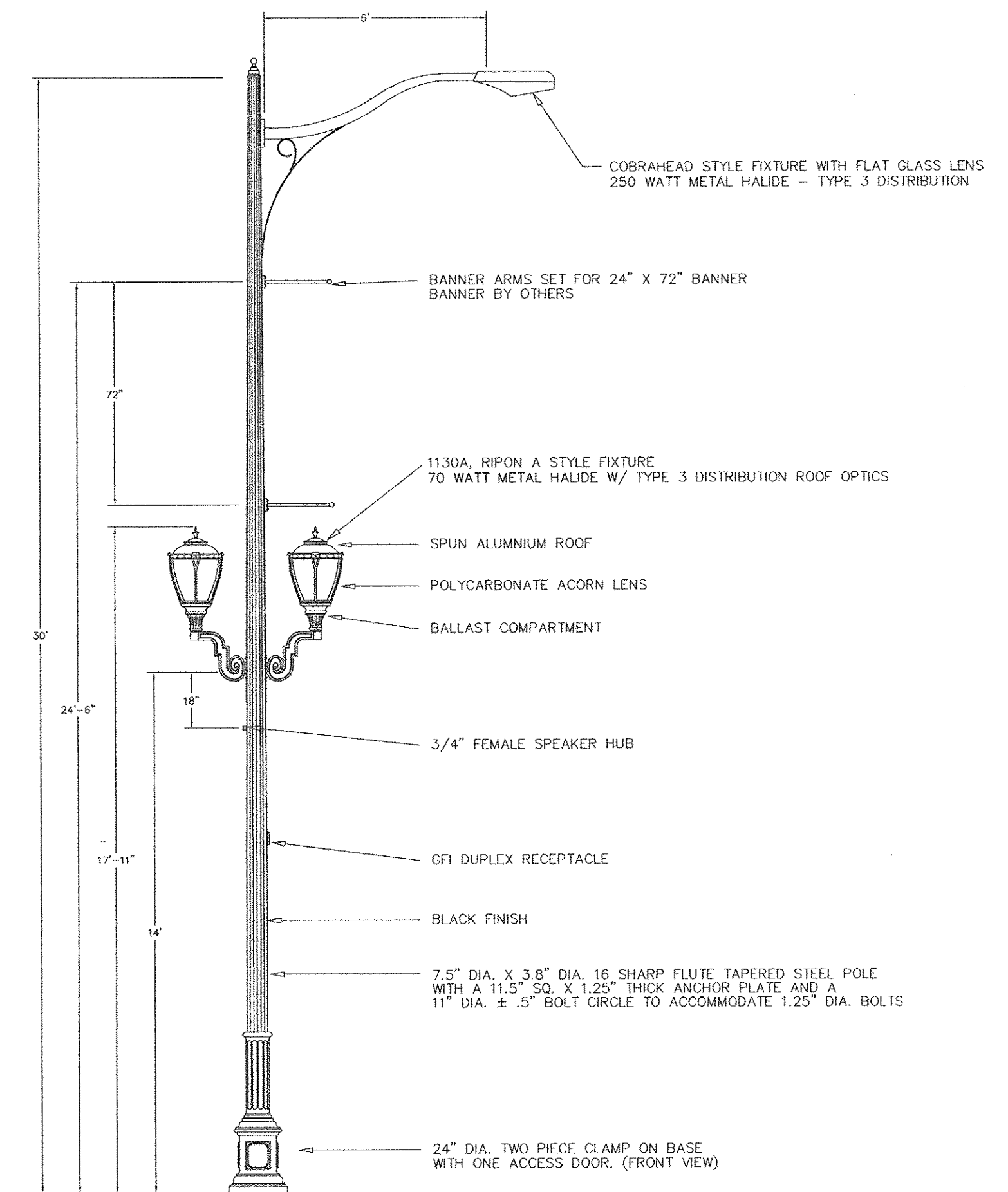
DATE	JUNE 2003
PROJ. NO.	HA0247
FILE NO.	HA024705
SHEET	9 OF 12







LIGHT POLE, ALUMINUM, 14'
N.T.S.



LIGHT POLE, ALUMINUM, 30'
N.T.S.

